

SOUTH 9TH STREET CORRIDOR PLAN

PUBLIC DRAFT
October 2024





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The South 9th Street Corridor Plan was funded by a Caltrans Sustainable Communities Grant.

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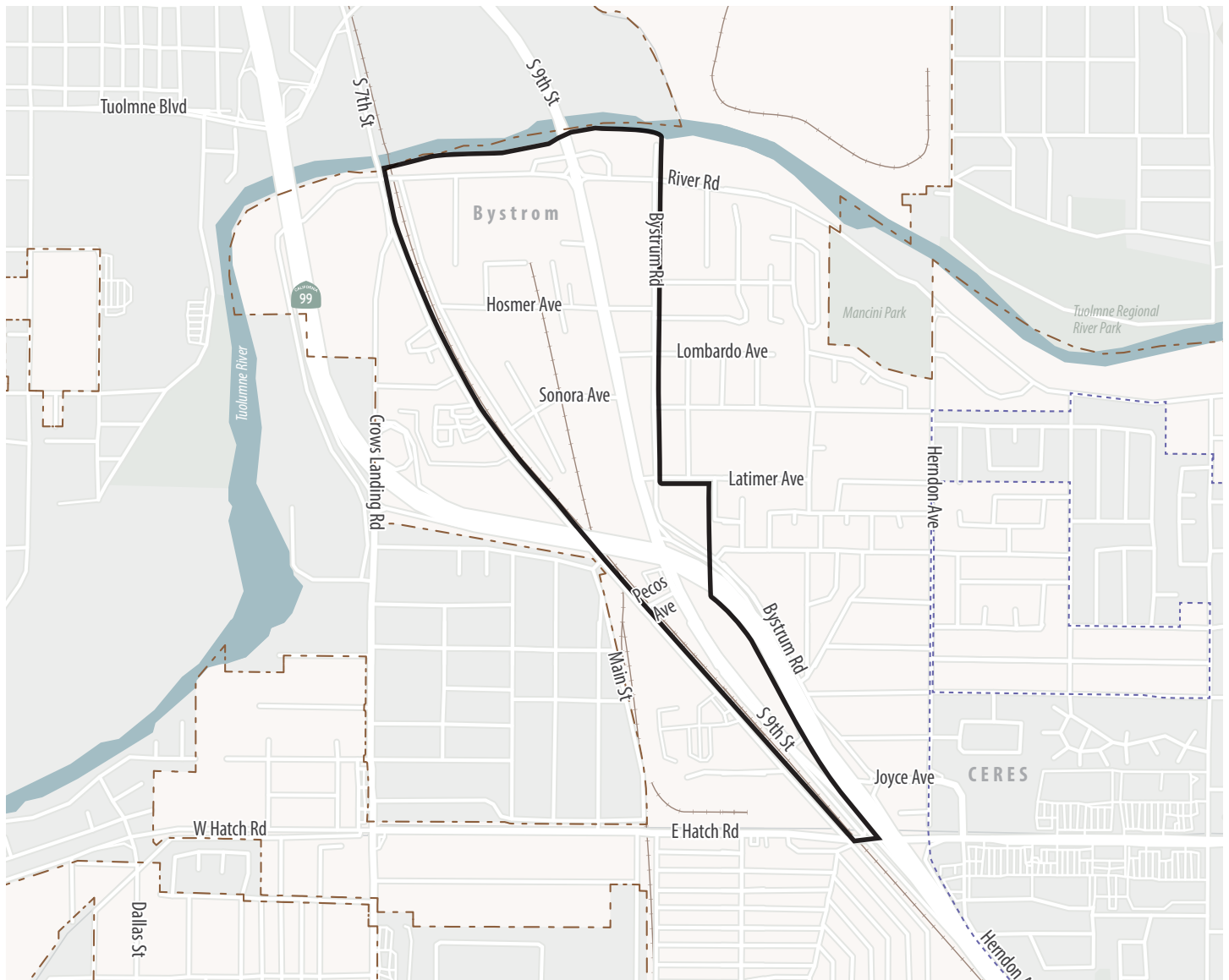
Chapter 1:
Introduction

The Corridor Plan Area

The South 9th Street Corridor Plan Area encompasses approximately 200 acres in Stanislaus County surrounding the South 9th Street Corridor. The Plan Area is defined by the Tuolumne River to the north, East Hatch Road to the south, South 7th Street to the west, and Bystrum Road and State Route 99 to the east, as shown in **Figure 1** below. The Plan Area is within the Local Agency Formation Commission’s adopted Sphere of Influence and is served by City of Modesto’s sewer and water infrastructure and Turlock Irrigation District’s electrical infrastructure.

South 9th Street is the primary corridor connecting the Cities of Modesto and Ceres due to the limited number of Tuolumne River bridge crossings and serves as a gateway into Downtown Modesto. Within the Plan Area, it is the predominant roadway in the north-south direction. A set of collector streets in east-west direction, including River Road, Hosmer Avenue, Lombardo Avenue, Latimer Avenue, and Hatch Road intersect South 9th Street and connect the Plan Area to surrounding neighborhoods.

Figure 1: South 9th Street Plan Area



Purpose & Guiding Principles

The Corridor Plan is intended to guide public improvements and infill growth along South 9th Street and throughout the Corridor Plan area. The document has been crafted from the generous input of community residents, business representatives, and agency staff and is based on their goals and priorities.

The Corridor Plan recognizes the importance of South 9th Street as a regional resource serving multiple jurisdictions and represents a comprehensive planning strategy promoting infrastructure improvements and economic growth.

The Corridor Plan seeks to achieve these goals by emphasizing the following principles:



Increase and improve connections to and from South 9th Street.



Improve roadway safety for people walking, biking, driving, and riding transit.



Provide comfortable facilities for walking and biking by delineating users in space.



Build green infrastructure such as rain gardens and green stormwater retention to help alleviate ponding, treat stormwater, and provide landscape opportunities.



Improve the appearance of the corridor by enhancing existing properties and providing landscape, streetscape, and façade improvements.



Support new land uses along the corridor such as commercial, retail, cafes, restaurants and housing that serve the needs of residents and employees.



Increase the sense of safety by following principles of Crime Prevention Through Environmental Design for public and private properties.

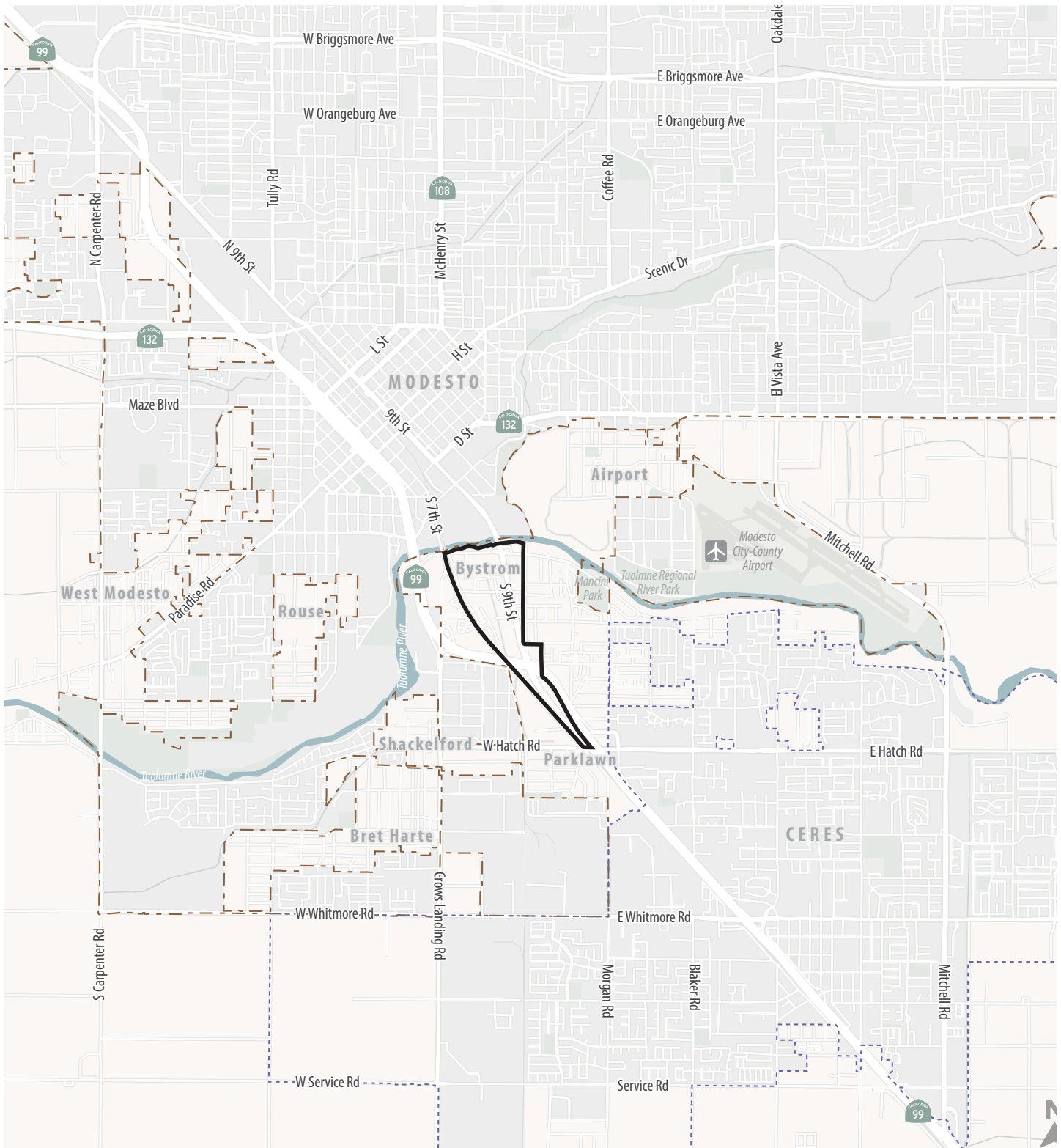
Why South 9th Street?

South 9th Street is a five-lane arterial connecting downtown Modesto to State Route 99 and Ceres, as shown in **Figure 2**. The corridor is characterized by a mix of commercial and industrial uses, including auto shops, small retail stores, and warehouses. The corridor serves as a major thoroughfare for local businesses and residents, though it has seen limited new development and lacks key infrastructure such as sidewalks. Many people walk, bicycle, and take the bus along the corridor, but the design of the street prioritizes vehicle throughput and speed.

The South 9th Street Corridor Plan will set the vision for transforming South 9th Street and surrounding vicinity into a safe and convenient multi-modal corridor that will support a variety of land uses, promote new development, and improve the land use compatibility of existing uses. This Plan builds on the previous planning efforts by Stanislaus County, City of Ceres, City of Modesto, Stanislaus Regional Transit Authority (StanRTA), and Altamont Community Express (ACE) Rail.



Figure 2: Regional Context



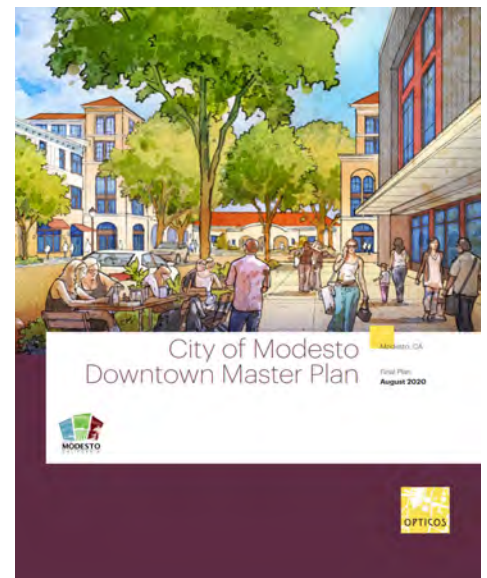
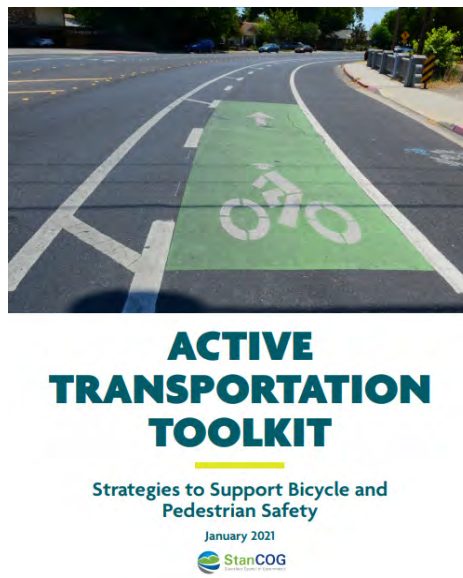
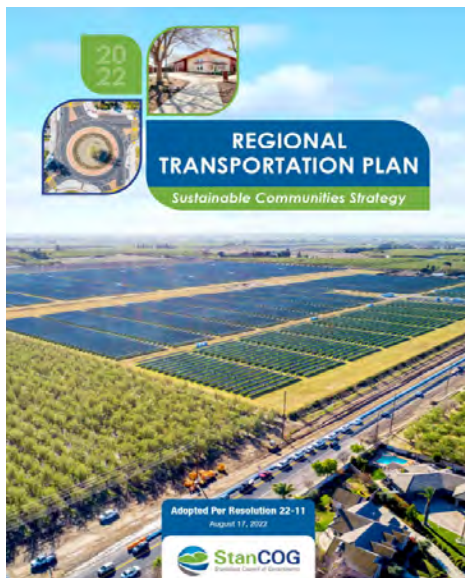
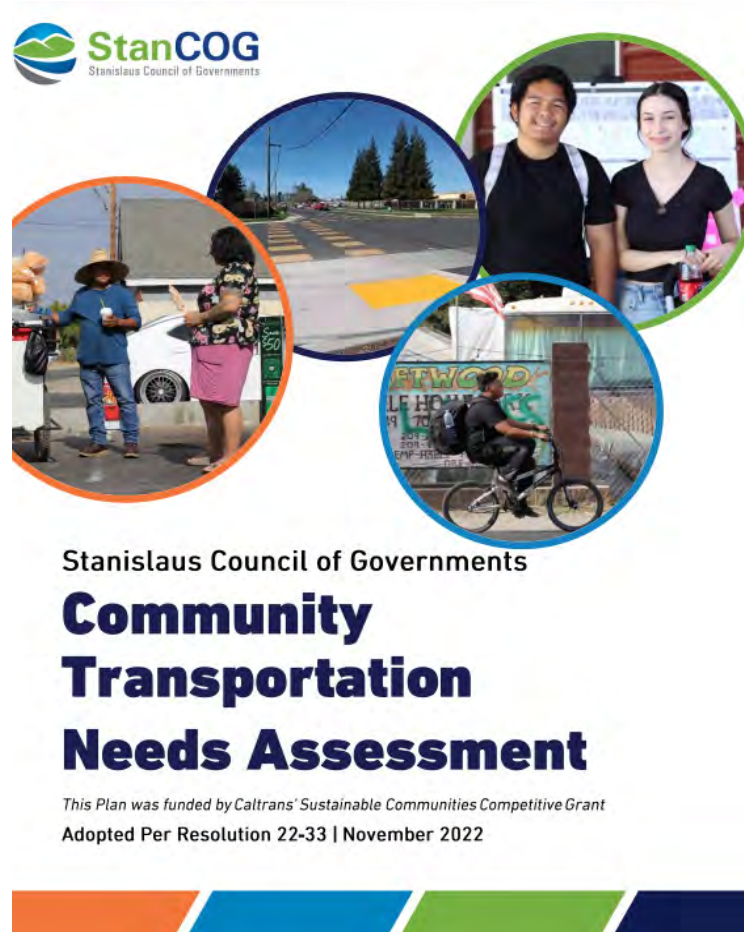
-  Plan Area
-  Modesto City Limits
-  Ceres City Limits
-  Unincorporated Area

Relevant Plans & Policies

The following plans and policies were reviewed to inform our work and support plan consistency:

- StanCOG Regional Transportation Plan/ Sustainable Community Strategies
- StanCOG Non-Motorized Transportation Master Plan
- StanCOG Active Transportation Toolkit
- Stanislaus County General Plan
- Stanislaus County Local Road Safety Plan
- Bystrom Community Transportation Needs Assessment
- Modesto Systemic Safety Analysis Report
- Modesto Local Road Safety Plan
- Modesto Non-Motorized Transportation Master Plan
- Modesto Downtown Master Plan
- Ceres General Plan 2035
- Ceres Active Transportation Plan
- Tuolumne River Regional Park Master Plan and Projects

Summaries of these plans can be found in **Appendix A.**



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Chapter 2:

Existing Conditions

Existing Conditions

This chapter presents an overview of the existing conditions of land use and transportation in the South 9th Street Plan Area. Additional documentation on existing conditions can be found in **Appendix B**.

General Plan Land Use

To analyze opportunities for land use changes that will improve the character of the South 9th Street Corridor, it is important to understand the land use designations that influence the development of the Plan Area. The Stanislaus County General Plan defines these designations, serving as a guiding framework for the area's development. By assigning specific land uses to each parcel, jurisdictions can establish development policies and regulatory guidelines that shape the area's overall character.

Figure 3 illustrates the land use designations from the Stanislaus County General Plan. The South 9th Street Corridor Plan Area is within the jurisdiction of Stanislaus County. However, the Plan Area is also influenced by surrounding land use designations from the City of Modesto General Plan and City of Ceres General Plan. The Plan Area is located within the Local Agency Commission (LAFCO) adopted Sphere of Influence of the City of Ceres. Under the County's General Plan, any projects requiring discretionary approval within the Plan Area (e.g. use permit or rezone) would require support from the City of Ceres.

The majority of the Plan Area is designated by the County for commercial land uses, except for the northwest section which is designated for industrial uses. The General Commercial designation is intended for various forms of light to heavy commercial uses, including retail, service, and wholesaling operations. This designation also allows for residential development in limited situations and when connected to both public sewer and water service. The Industrial designation allows for light or heavy industrial uses, including, but not limited to, manufacturing and warehousing.

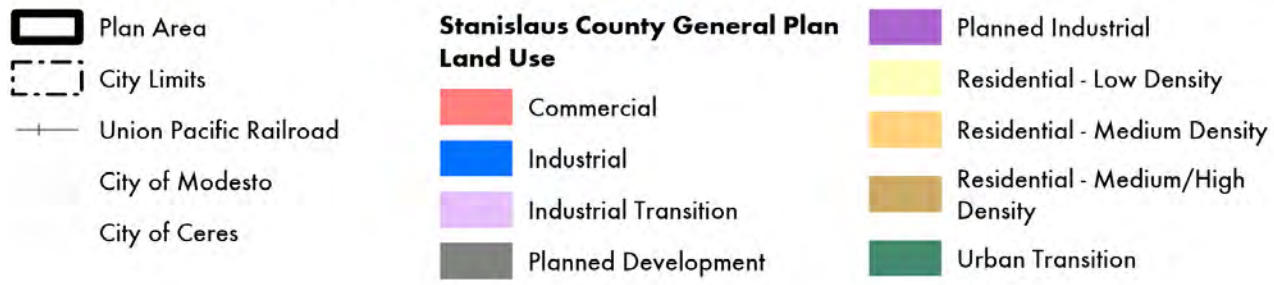
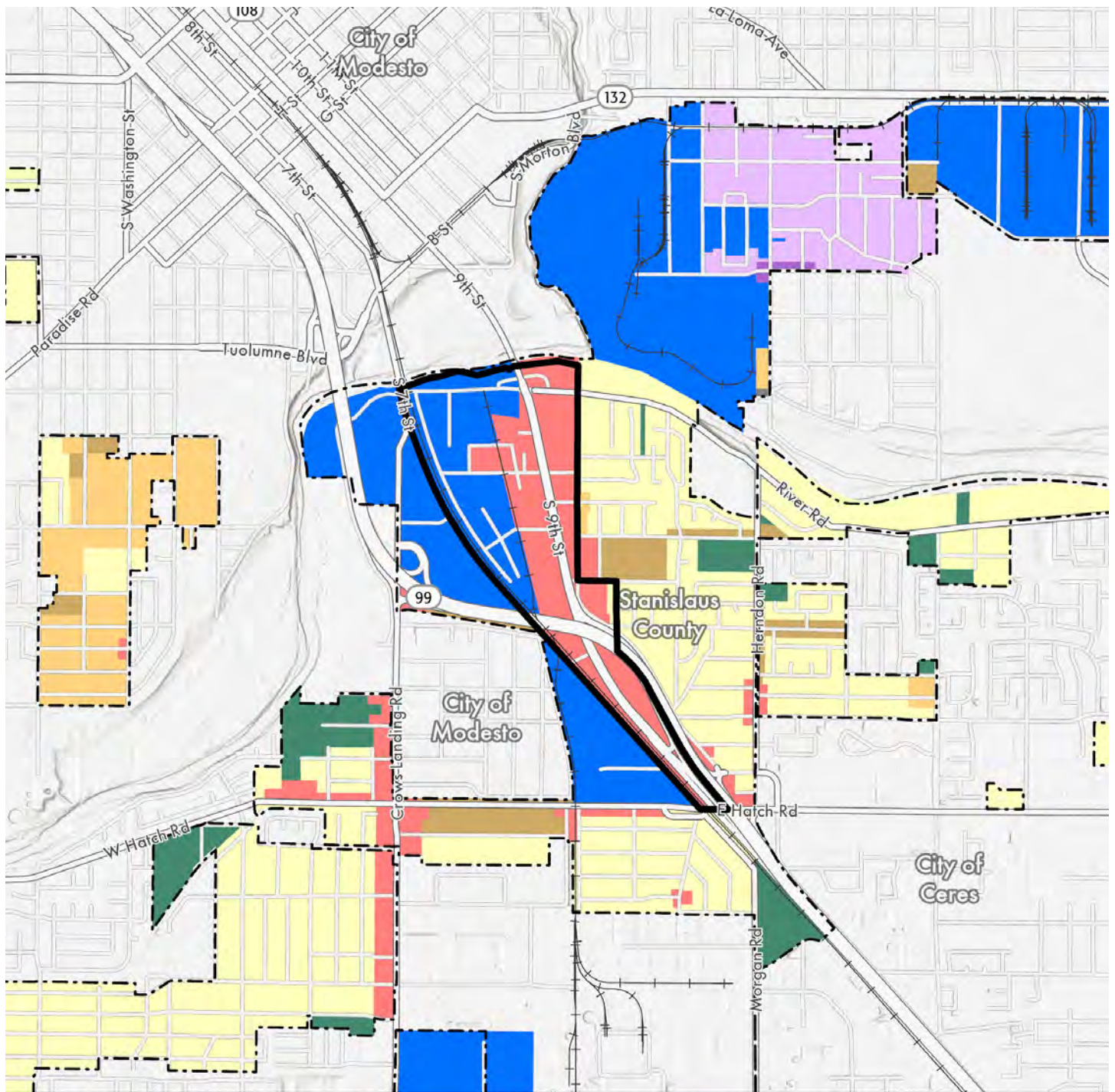
Stanislaus County's 6th Cycle Housing Element Update is proposing to rezone the area southeast of the Bystrum Road/Latimer Avenue intersection to medium to high density residential use.

The area north of the Plan Area, across the Tuolumne River, is within the City of Modesto, and is characterized by the open space of the Tuolumne River Regional Park, serving as a buffer zone between Downtown Modesto and the Plan Area. The area immediately west of the Plan Area is primarily designated by the County for industrial land uses.

The southern boundary of the Plan Area is at East Hatch Road, with the area southeast falling within the City of Ceres and designated for Commercial uses. The area to the southwest of SR 99, south of Hatch Road, is part of Stanislaus County and contains Low-Density Residential and Commercial land uses.

The land east of the Plan Area, still within Stanislaus County, includes Low-Density Residential, with smaller pockets of Commercial, Medium-Density, Medium/High-Density Residential and Urban Transition land uses.

Figure 3: General Plan Land Use



Zoning Designations

A jurisdiction's zoning ordinance consists of a set of regulations and guidelines that govern the use and development of land and properties within its boundaries. While a general plan serves as an overarching policy document, zoning ordinances provide the legal framework and specific regulations for day-to-day land use and development decisions. The Stanislaus Zoning Ordinance states the permitted, conditionally permitted, and prohibited uses within each zoning district. **Figure 4** illustrates the existing County zoning in and around the Plan Area. Within the Plan Area, the west and northwest portion of the Plan Area is zoned for Industrial (M) use. Most of the land immediately east and west of South 9th Street is zoned for General Commercial (C-2), although the overall development can be characterized as more intensively industrial than commercial.

Single Family Residential (R-1), Medium Density Residential (R-2), and Multiple Family Residential (R-3), uses are to the east of the Plan Area. The area immediately west of the Plan Area is zoned for Industrial (M).

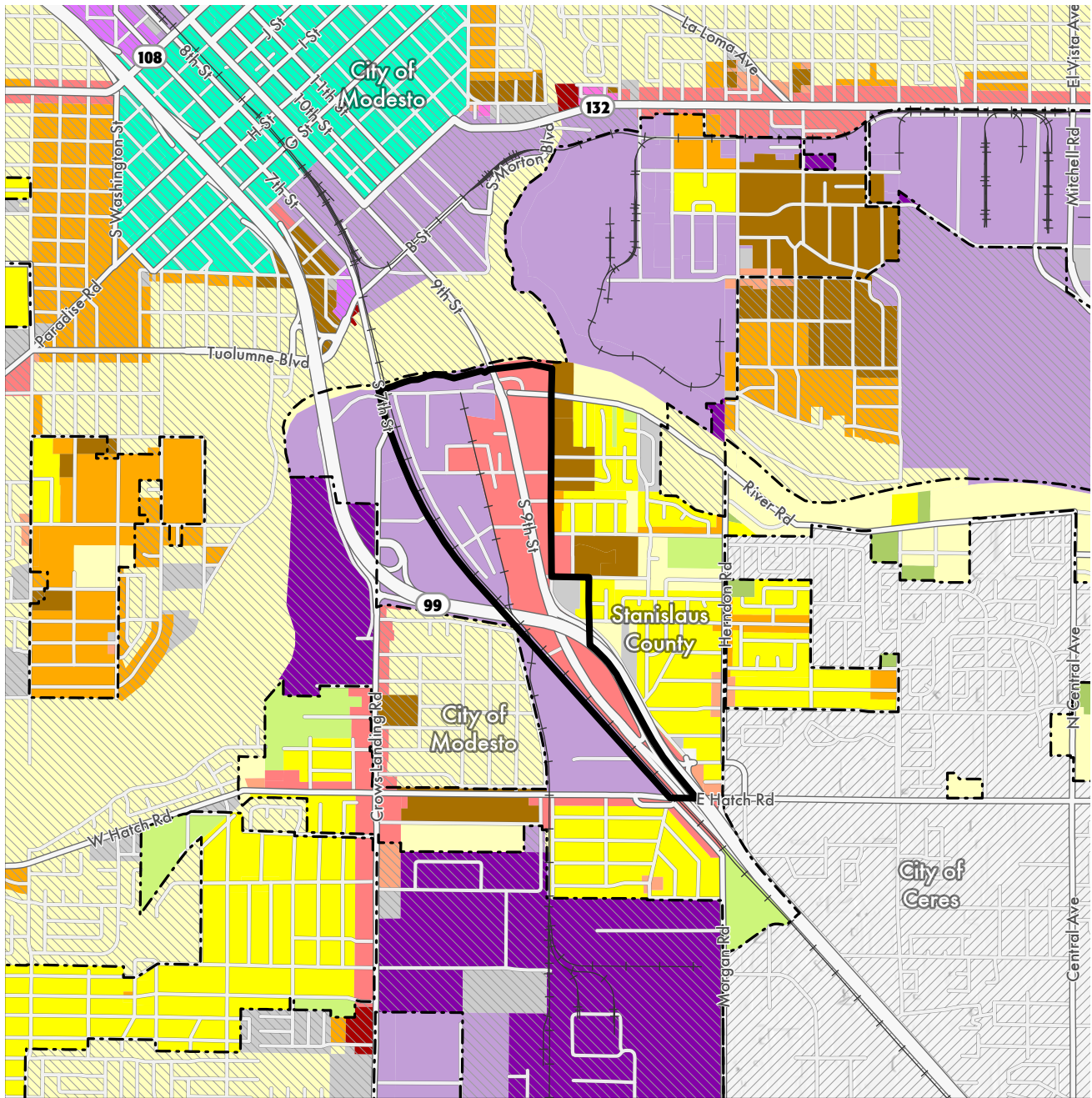
Zoning Ordinance Requirements

All new buildings in the General Commercial (C-2) and Industrial (M) zones are required to be consistent with the Zoning Ordinance development standards for setbacks, landscaping, height, parking, and other requirements. Throughout the Plan area there are many legal nonconforming buildings and parcels that don't meet current zoning requirements, and land uses (such as wrecking yards) that are reflective of the timeframe in which the corridor has developed.

The C-2 zone allows for public facilities, churches, daycare, community centers, wholesale and retail stores, and other commercial uses. Single-family dwellings or one apartment is permitted as an accessory to permitted commercial use. A permit is also required for uses related to certain types of manufacturing, assembly of mechanical equipment, compounding and packaging of pharmaceuticals, retail or wholesale retail stores with gross building and sales area of 65,000 square feet or greater, commercial cannabis retail or testing activities, and drilling of natural materials. A maximum building height of 35 feet is allowed in the zoning districts.

The M zone permits all retail and wholesale establishments, warehouses, service establishments, public and quasi-public buildings; junkyards, wrecking yards, and auto dismantling yards, all uses permitted in the commercial zoning districts except dwelling units of any kind unless otherwise specifically permitted in this zone. Use permits are required for uses including distillation activities, manufacturing of acid or explosives, stockyards, or slaughterhouses, refining of petroleum products, drilling activities, all retail stores and wholesale retail stores with a gross building and/or sales area of 65,000 square feet or greater, emergency shelters, and cannabis-related activities. A maximum building height of 75 feet is allowed in the zone; however, there is no height limit for fireproof structures (excluding advertising structures) not used for human occupancy.

Figure 4: Zoning Designations



Plan Area	Medium Density Residential	Neighborhood Commercial
City Limits	Multiple Family	General Commercial
Union Pacific Railroad	General Commercial	Highway Commercial
City of Modesto	Neighborhood Commercial	Commercial-Industrial
City of Ceres	Industrial	Professional Office
Stanislaus County Zoning	Planned Development	Light Industrial
General AG 10 Acre UT	City of Modesto Zoning	Heavy Industrial
General AG 3 Acre UT	Low-Density Residential	Downtown Zones
Rural Residential	Medium-Density Residential	Planned Development
Single Family Residential	Medium-High-Density Residential	

*Ceres General Plan zoning data not available at time of preparation.

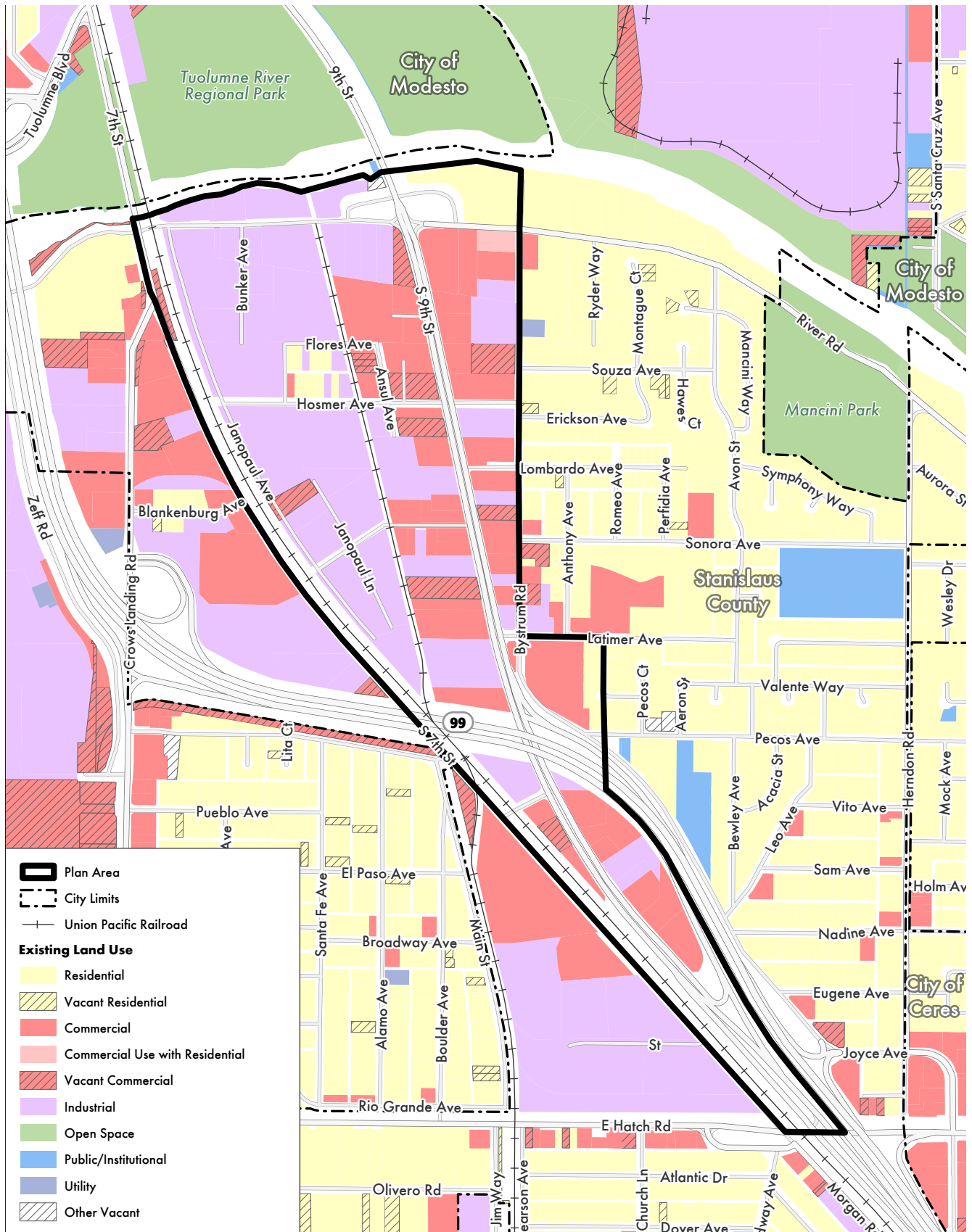
Existing Land Use

Figure 5 illustrates the existing land uses found within Plan Area and the land uses immediately adjacent to the Plan Area. The Plan Area is dominated by a mix of industrial and commercial land uses, including manufacturing uses, food processing, California Redemption Value (CRV) recycling centers, auto wrecking yards, truck driving schools, and warehouses. Uses such as retail stores, hotels, motels, lodges, auto-repair shops, car dealerships, tractor-trailer driving schools, sales offices, and commercial vacant land are distributed along South 9th Street. The intensity of industrialized uses and development increase towards Bystrum Road on the east and to South 7th Street on the west. Very few residential uses are present within the Plan Area. Primarily, residential land uses within the Plan Area consist of single-family residences and mobile home parks located along Flores Avenue and Hosmer Avenue west of South 9th Street, and River Road east of South 9th Street. As discussed in the General Plan Land Use section, this property is intended to be rezoned to medium high residential use as proposed by the County's 6th Cycle Housing Element Update.

The area immediately west of the Plan Area, north of State Route 99 and between 7th Street and Crows Landing Road, contains a mix of industrial, commercial, residential, and some vacant commercial and . The area to the west, south of State Route 99 and west of 7th Street, contains a mix of commercial and industrial uses with residential uses such as Shackleford Neighborhood, mainly occurring in the City of Modesto. The areas east of the Plan Area contains a residential neighborhood, Bystrum Neighborhood, with a few commercial uses.

Figure 5 also shows properties within the Plan Area that are underutilized or vacant. Many of the underutilized lots are currently used for vehicle storage and parking. These underutilized lots can be potential opportunity sites for new development.

Figure 5: Existing Land Uses



Source: City of Modesto, 2023; Stanislaus County Assessor's Data, 2023; PlaceWorks, 2024.

Key Destinations

Prevalent land uses within the South 9th Street Corridor Planning Area are auto-oriented industrial services, and warehouses. Notably, this planning area lacks commercial shopping centers, schools, libraries, parks, or major transit facilities within the immediate vicinity.

Within a Half Mile...

Several important business and service destinations are located within a half-mile radius of the Plan Area, as illustrated in **Figure 6**. Most people can walk a half mile in ten minutes, so it is frequently used by planners to check proximity to key destinations. Most notably within a half-mile, the Ceres Plaza Shopping Center, an auto-oriented retail hub, can be found just southeast of the Planning Area, near the intersection of East Hatch Road and SR 99. This shopping center features major retail and grocery stores such as Grocery Outlet, Cost Less Foods, The Home Depot, 99 Cent Stores, DD's Discounts, Big 5 Sporting Goods, and O'Reilly Auto Parts.

Two significant parks are located within the half-mile radius of the Plan Area. Mancini Park, managed by the City of Modesto, is on River Road just east of the Plan Area. The Tuolumne River Regional Park (TRRP), located north and northeast of the Plan Area, across the Tuolumne River, encompasses 500 acres of parkland following the Tuolumne River and is the largest urban park in Stanislaus County. The TRRP area north of South 9th Street is planned to be a new Gateway Park developed through a joint powers agreement with the City of Modesto, Stanislaus County, and City of Ceres who will be withdrawing from the JPA effective June 30, 2025.

Tuolumne Elementary School and Shackelford Elementary School, both part of the Modesto City School District, are also situated within this half-mile radius.

Within a Mile...

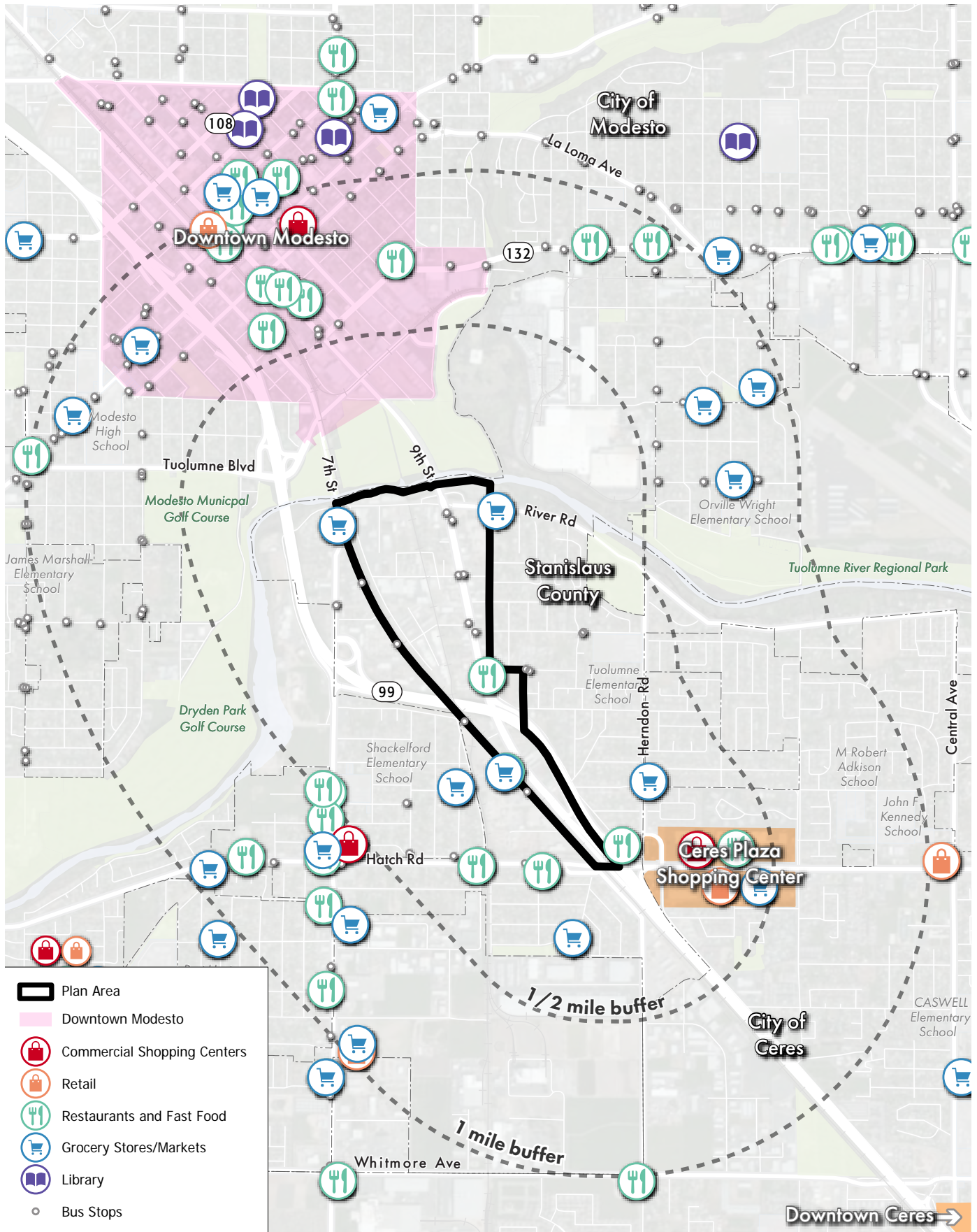
Within a one-mile radius of the Plan Area, the primary destination is Downtown Modesto to the north, a bustling urban center offering an array of key attractions, including the restaurants, offices, art galleries, movie theaters, retail stores, and other businesses. Downtown Modesto also provides convenient access to public transit, with the Modesto Transit Center serving as a central hub connecting the bus and transit system.

Beyond a Mile...

Beyond the one-mile radius of the Plan Area, there are several other noteworthy destinations. Civic centers for the City of Modesto and Stanislaus County, as well as the Stanislaus County Courthouse and the Stanislaus County Library are situated within Downtown Modesto. Modesto Junior College, also located to the north, serves as the nearest higher education facility to the Plan Area.

Downtown Ceres, situated southeast of the planning area, features a diverse mix of neighborhood and regional commercial spaces, along with grocery stores offering fresh food and corner stores. Downtown Ceres also includes a Community Center and a branch of the Stanislaus County Library.

Figure 6: Key Destinations



Existing Roadway Network

South 9th Street includes four wide travel lanes with a central median and narrow walking areas on either side, as shown in the illustrative cross-section in **Figure 7**.

Figure 8 displays the roadway network in the Plan Area. Most roadways within the Plan Area have two lanes with South 9th Street being the exception. Most intersections along South 9th Street are side-street stop controlled, except for the signalized intersections of Latimer Avenue/South 9th Street and Pecos Avenue/South 9th Street. To the east of the Plan Area, River Road, Souza Avenue, Lombardo Avenue, Sonora Avenue, and Lombardo Avenue provide connections to neighborhoods on the east side of Bystrum Road. An overview of the existing right-of-way conditions, which highlight issues within the pedestrian and bicyclist infrastructure within the Plan Area, is provided in **Appendix B**.

Traffic Mix & Volumes

South 9th Street sees a mix of passenger vehicles, light-duty trucks, vans, trucks with trailers, and semi-trucks totalling over **18,500 vehicles daily**. In the AM peak hour (7:30 - 8:30 am), over 1,300 vehicles travel along the corridor, and in the PM peak hour (4:30 - 5:30 pm), over 1,600 vehicles travel along the corridor.

Along with heavy peak commuter traffic, South 9th Street serves as a major truck route, with over 7% of daily vehicle traffic being heavy-duty.

This diverse mix of vehicles, pedestrians, and bicyclists can create conflicts between roadway users as they compete for space both on the road and along sidewalks and driveways. Between 2017 and 2022, the corridor has experienced over 90 collisions. The factors that contribute to these collisions vary but can generally be attributed to the existing roadway design, signals, and the diverse composition of roadway users. In depth collision data analysis is provided in **Appendix B**.

Railroad Network

Union Pacific Railroad tracks are located on the western edge of the Plan Area and run parallel to South 7th Street. The Alamont Corridor Express extension from Lathrop to Ceres will build a second track along this section of rail (discussed in more detail on page 28). A non-operational segment of the Union Pacific Railroad is also present in the industrial area between South 9th Street and Janopaul Avenue.

Figure 7: Existing Cross-section of South 9th Street between River Road and Latimer Avenue

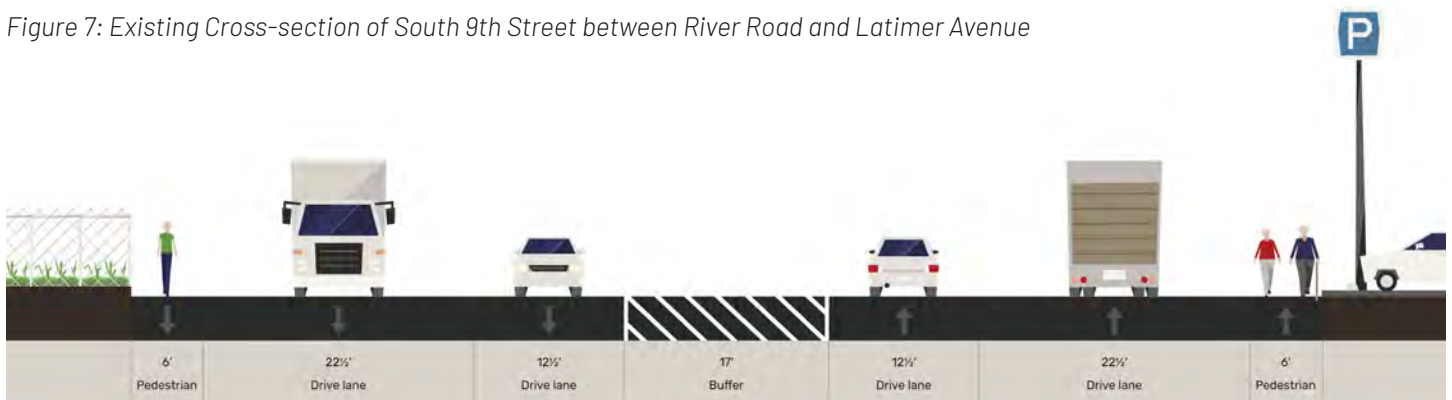
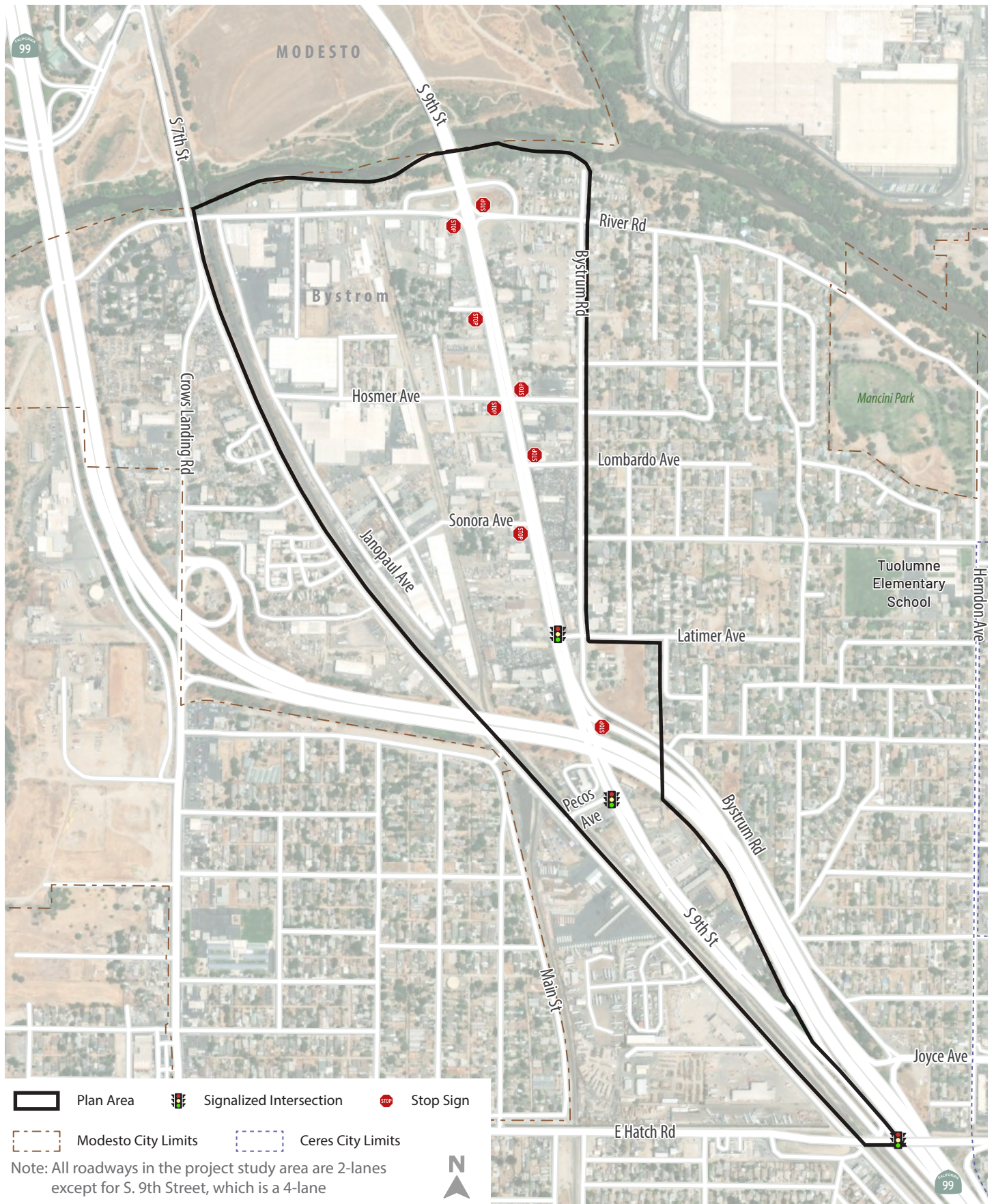


Figure 8: Roadway Network



Existing Walking & Biking Facilities

Figure 9 displays the pedestrian and bicycle facilities located in the Plan Area. Observations along the corridor indicated that walking and biking are relatively common. Pedestrians were observed frequently crossing mid-block and at unmarked locations.

Painted white lines along much of the corridor serve as a delineation between the County's Right-of-Way and private property, and define the path of travel for pedestrians. These areas differ in quality from gravel to evenly paved concrete areas, and most areas designated for walking have uneven pavement. Several segments of the walking network are blocked by parked vehicles, causing pedestrians to go into the road to pass. With a few exceptions, the corridor is not compatible with Americans with Disabilities Act (ADA) Standards for Accessible Design due to pavement unevenness, narrow clearance widths, lack of curb ramps, and other hinderances to a clear path of travel.

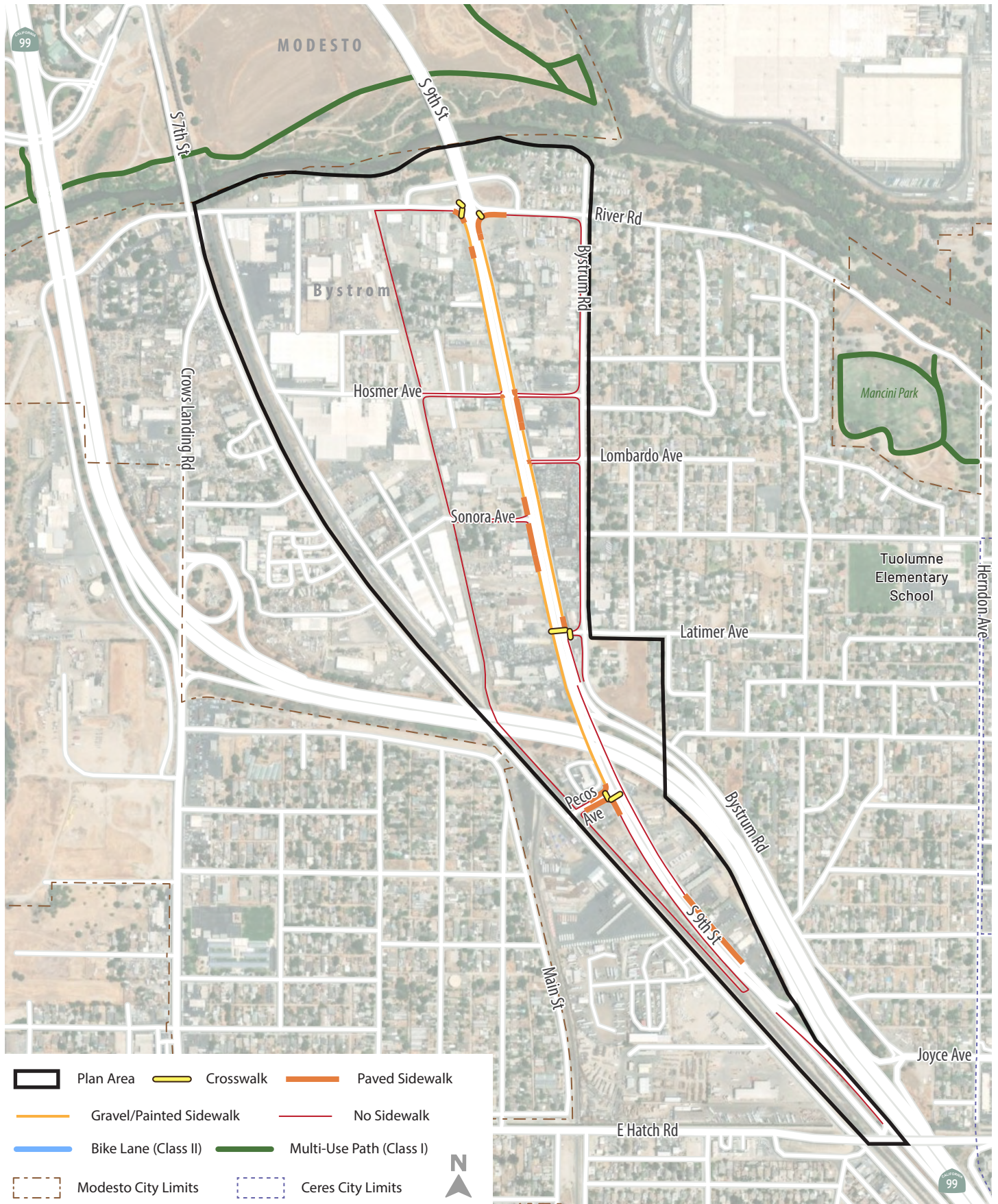
The Plan Area lacks a tree canopy. Street trees can lower temperatures, calm traffic, reduce particulate matter, and create aesthetic beauty, all of which contribute to a comfortable environment for walking and biking.

Most of the intersections in the Plan Area lack crossing facilities.

There are currently no bike facility options within the Plan Area. Directly north of the South 9th Street bridge is the Tuolumne River Bike Trail, within the Tuolumne River Regional Park, which accommodates both bicyclists and pedestrians. The nearest bike facilities can be found along East Hatch Road southeast of the Plan Area in Ceres.



Figure 9: Bicycle and Pedestrian Facilities



Existing Transit Facilities & Service

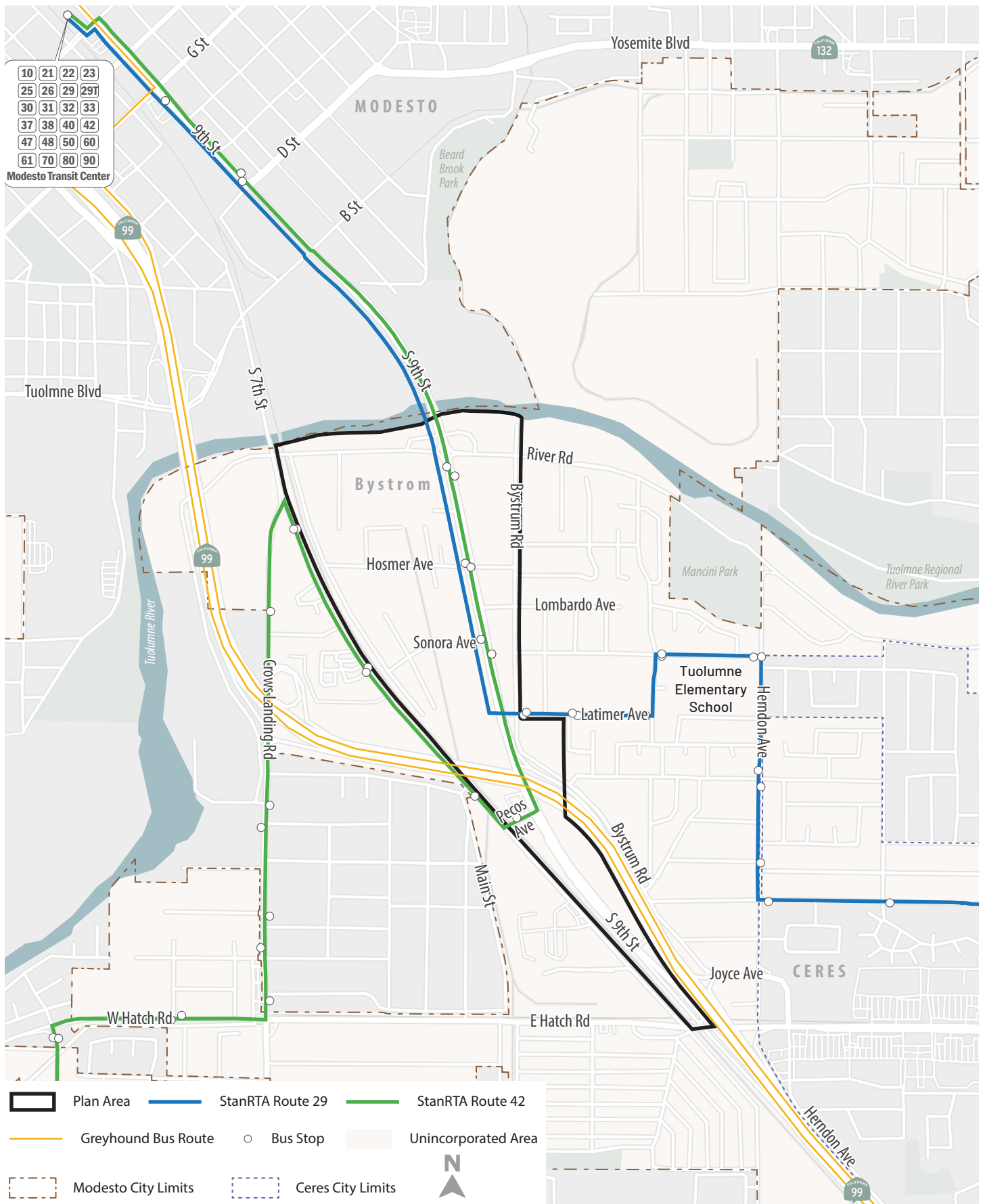
Figure 10 displays the transit service and facilities located in the Plan Area. The Stanislaus Regional Transit Authority (StanRTA) provides bus service including nine bus stops throughout the Plan Area. Bus stops for Routes 29, 29T, and 42 are located along South 9th Street and South 7th Street. These bus routes provide interregional service between downtown Modesto and Ceres. StanRTA staff report medium to high regular ridership along the corridor in 2024.

Most bus stops in the Plan Area do not have bus shelters, except the bus stop located at South 9th Street and River Road.

Regional connectivity provided by Amtrak, Greyhound, and StanRTA services requires transferring from stops within the Plan Area to the Downtown Modesto transit hub.



Figure 10: Transit Facilities



Recent & Anticipated Work in the Plan Area

There are a variety of upcoming capital improvement projects that were considered during the South 9th Street planning process. These include new passenger rail service, upcoming pavement rehabilitation, and upgrades to the nearby 7th Street Bridge.

Altamont Corridor Express (ACE) Extension

ACE is participating in a joint program with the Amtrak San Joaquins rail line known as Valley Rail, which includes rail improvement and expansion between cities in the San Joaquin Valley and Sacramento.

The ACE Lathrop to Ceres Extension Project is wrapping up the final engineering design phase, with some construction having started in Lathrop. One of the components of the Valley Rail program is the Modesto Station Area Partnerships, which will implement infrastructure improvements for downtown Modesto and provide transit-oriented

development in the vicinity of the Modesto Transit Center, which comes in anticipation of ACE serving Modesto by 2026. The Modesto Transit Center is located on 9th Street, 1.5 miles north of the Plan Area. Given the proximity of the Modesto Station Area Partnerships, there is potential for the project to provide greater transit access and economic benefit to the Plan Area. Within the Plan Area, ACE is expanding the rail lines to include a second track running parallel to the existing rail line, which will impact the at-grade crossing at Pecos Avenue. At the 7th Street & Pecos Avenue intersection, improved pedestrian crossings are slated for construction.



Expanded ACE Train Service Network

9th Street Pavement Rehabilitation Project

Prior to the Corridor Plan process, Stanislaus County Public Works staff began developing a pavement rehabilitation project on the South 9th Street Corridor. The project developed draft plans, however due to funding constraints, the project was put on hold.

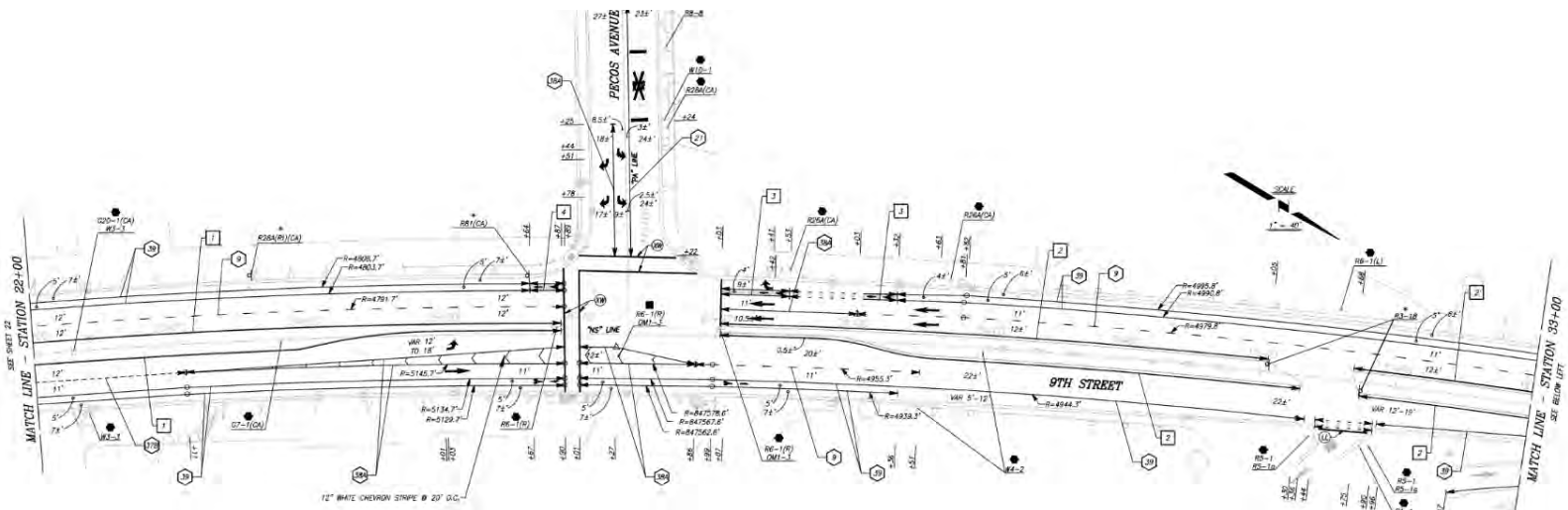
The plan includes the majority of South 9th Street covered in the Plan Area. Along with the pavement rehabilitation, key improvements include:

- Striping with bike lanes
- Construction of ADA compliant curb ramps at intersections
- Installing a material to reinforce the pavement where reflective cracking exists or could potentially occur.

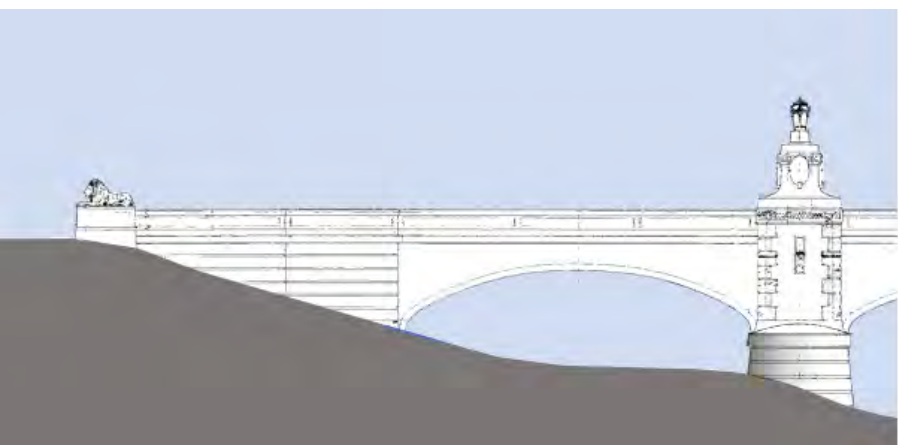
South 7th Street Bridge Re-Construction

The South 7th Street Bridge is approximately a third of a mile west of the South 9th Street Bridge. South 7th Street is a parallel route into downtown Modesto. However, the South 7th Street Bridge is currently weight restricted to 4 tons and not suitable for truck traffic.

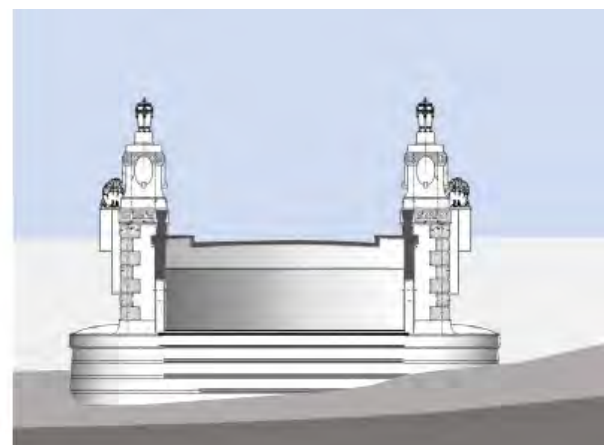
Structural updates will be made to the bridge, with construction expected to occur in 2025 and beyond. This update will allow heavier trucks to access the route, which may have a redistributing effect on truck traffic on South 9th Street.



9th Street Pavement Rehabilitation Project Design



South 7th Street Bridge Re-Construction Design



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S 9th St

Site Health

Farmer's Market
Fruitstand
Fruit Market

Home
Park
Park

Vehicle
Mobility
on 9th

Paul Ave



Chapter 3:

Community Engagement

Community Engagement Overview

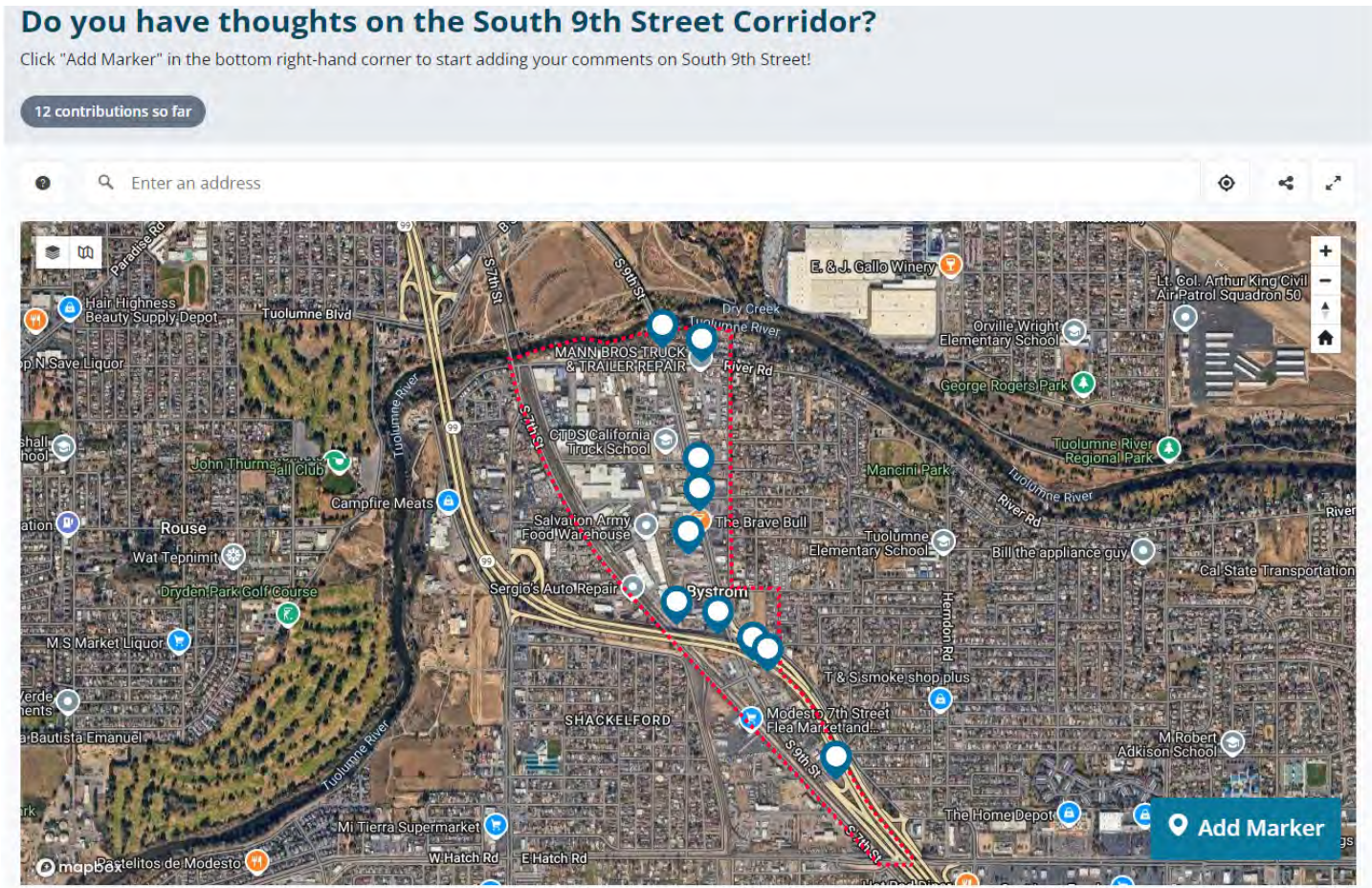
The South 9th Street Corridor Plan was developed through a public engagement process that engaged businesses along the corridor, residents living in or near the Plan Area, as well as other stakeholders. Community input and involvement were crucial to understanding issues and concerns, identifying ways to improve the experience of travelers of all transportation modes using the corridor, enhancing the interface between commercial, industrial, and residential land uses, and supporting infill development.

Community Engagement Strategies

The five primary community engagement strategies utilized for the Corridor Plan were community events (workshops, stakeholder meetings, walk audits, and pop-ups), the Project Advisory Group, flyers and social Media announcements, a business-focused survey, and other online engagement. These strategies informed the public about the Corridor Plan while actively engaging community members and stakeholders and allowing them to provide meaningful input.

Project Website

A project website was created and updated throughout the planning process with up-to-date project information, documents, announcements about recent and upcoming events, and links to the community survey. An interactive comment map was created as a supplemental input for residents and stakeholders to highlight location-specific challenges and opportunities within the Plan Area. All points were automatically geo-referenced, allowing people to see where individuals had issues or ideas.



Flyers and Social Media Announcements

The project included a variety of outreach methods, including printed media and an online presence. Bilingual flyers were created to promote the harrette activities since, according to the U.S. Census Bureau American Community Survey 5-Year Estimates from 2018-2022, demographics for the Plan Area’s corresponding census tract were identified as 76% primarily Hispanic or Latino. Both illustrative and online outreach materials were created in English and Spanish. Flyers were distributed directly to businesses on the corridor and mailed to residences within the Plan Area.

In addition to this, the project team developed social media messages, including posts for Facebook and Instagram, to reach out to interested members of the community. Meeting notices and other communications were sent via email blast to notify the stakeholders about upcoming meetings and project updates. The County also used its electronic billboard to advertise events and workshops in English and Spanish. The County also advertised events and workshops via the parent portal utilized by Tuolumne Elementary student population and via the County’s electronic billboard visible to traffic on southbound State Highway 99 near the Hatch Road off-ramp.

Business Survey

A survey was created and distributed to businesses from February to May 2024 to identify some of the issues and needs that the project team should focus on to address problems and support business within the Plan Area. The survey solicited information about types of infrastructure investments, traffic safety issues, and challenges for pedestrians, bicyclists, or motorists in accessing their businesses. Also asked were questions regarding which types of infrastructure investments, additional business types, and types of housing businesses would like to see in the corridor. While surveys were distributed to all properties along the corridor in both English and Spanish, an online version was also made available. Thirteen businesses on the corridor returned the survey and a summary of the survey results can be found in Appendix D.



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT
1010 10th Street, Suite 3400, Modesto, CA 95354
Planning Phone: (209) 525-6330 Fax: (209) 525-5911
Building Phone: (209) 525-6557 Fax: (209) 525-7759

South 9th Street Corridor Plan Business Survey

Stanislaus County is seeking feedback from local businesses and property owners near South 9th Street to help guide efforts to identify and improve issues within the area. Your input is valuable and requested!

Kindly return this survey to the County Planning Department at 1010 10th Street, Suite 3400, Modesto, CA 95354 by **February 21, 2024**. You can also fill out an online survey instead by visiting [bit.ly/S9thStSurvey] in your web browser or scanning the QR code at the bottom of this form.

Name of business: _____

Address: _____

Type of business: _____ Size (# of employees): _____

Name and Title: _____

1. Do you own or rent your business location near South 9th Street? Own Rent

2. How would you describe the majority of your business’ customer base?

- Nearby residents
- Local businesses or employees
- People passing through
- Other: _____

3. How do your **customers** get to your business?

- Drive in a passenger vehicle (e.g. sedan, SUV) or light-duty truck (e.g. F150)
- Drive in service vehicle (e.g. semi-truck, delivery van, etc.)
- Walk
- Bus
- Bike
- I don’t have customers
- Other: _____

4. How do your **employees** get to your business?

- Drive in a passenger vehicle (e.g. sedan, SUV) or light-duty truck (e.g. F150)
- Drive in service vehicle (e.g. semi-truck, delivery van, etc.)
- Walk
- Bus
- Bike
- I don’t have employees
- Other: _____

5. How would you rank the traffic safety along South 9th Street? (e.g., speeding, dangerous crossings, etc.)

- Very unsafe
- Fairly unsafe
- Mostly safe
- Very safe

Project Advisory Group

The Project Advisory Group (PAG) was a key component of the community engagement process. The County and project team assembled a Project Advisory Group (PAG) as the lead group of residents and stakeholders to guide the team on the best ways to engage the public and discuss the plan concepts. The PAG consisted of members who represented corridor businesses and business interests, nearby schools, County departments, and City departments from Modesto and Ceres. Participating organizations included:

- City of Ceres
- City of Modesto
- Corridor business owners
- Modesto City Schools
- Opportunity Stanislaus
- Residents

- South Modesto Municipal Advisory Council
- Stanislaus Council of Governments (StanCOG)
- Stanislaus County Board of Supervisors
- Stanislaus County Community Services
- Stanislaus County Planning and Community Development
- Stanislaus County Public Works
- Stanislaus County Sheriff's Department
- Stanislaus Regional Transportation Authority (StanRTA)
- Tuolumne Elementary School

The PAG met three times during the project to share information, collaborate, and guide community engagement and development of the Corridor plan recommendations and guidelines.



Community Design Charrette

In March 2024, the project team conducted a multi-day community design charrette to actively engage community members, businesses, and stakeholders in the South 9th Street Corridor. The purpose of the charrette was to identify preliminary issues and locations to be addressed in the plan and to develop initial infrastructure recommendations. In consultation with the PAG, the project team hosted multiple activities to engage residents and stakeholders, including stakeholder meetings, a community design workshop, walk audit, and pop-up event.

YOU'RE INVITED!

Stanislaus County wants to hear from you on how to improve the experience along South 9th Street.

We want to hear from you:

- What are the challenges for the corridor?
- How can the County enhance the corridor visually and economically?

Food and refreshments provided! Children are welcome!

RSVP:
bit.ly/S9thStPlan

6-8 PM | **Corridor Community Workshop**
19 March | Tuzlumne Elementary School, 707 Herndon Rd.

9-10:30 AM | **Corridor Walking Audit**
20 March | Meet at the corner of Bystrom Rd & Latimer Ave.

CONTACT US
Planning and Community Development | 209.525.6330

Stanislaus County

¡ESTÁS INVITADO!

Ayuda al condado de Stanislaus a mejorar la experiencia a lo largo del corredor South 9th Street.

Cuéntanos:

- ¿Qué cambios te gustaría ver en esta zona?
- ¿Qué problemas hay en el corredor?
- ¿Qué puede hacer el Condado para mejorar el corredor visual y económicamente?

¡Se proporcionan refrigerios! ¡Los niños son bienvenidos!

PARA CONFIRMAR TU ASISTENCIA:
bit.ly/S9thStPlan

6-8 PM | **Taller Comunitario del Corredor**
19 Marzo | Tuzlumne Elementary School, 707 Herndon Rd.

9-10:30 AM | **Auditoria Peatonal**
20 Marzo | Punto de reunión en la esquina de Bystrom Rd y Latimer Ave.

LLÁMANOS AL
Planificación y desarrollo comunitario | 209.525.6330

Stanislaus County

Stakeholder Meetings

Three sessions for focus group-style meetings were held during the charrette and conducted to foster candid discussions around topics crucial to helping the team understand current conditions and potential improvements. During these meetings, participants helped identify their main concerns or issues that should be addressed during this process, help understand which bicycle- and pedestrian-friendly modifications for the corridor would be feasible, and issues around truck and transit access. These were also opportunities to discuss infrastructure and additional improvements to support the current corridor experience as well as the changing nature of the corridor.

These meetings were held on March 19, 2024, and included the following:

1. Agency Focus;
2. Community Services and Advocacy; and
3. Local Businesses.

Additional details from these meetings will be available in Appendix D.

Community Design Workshop

On March 19, 2024, the project team facilitated a workshop at Tuolumne Elementary School to engage community members. The workshop included a visioning exercise and a series of presentations and activities to solicit community opinions. The first presentation highlighted the various tools and strategies for making streets comfortable and safe for pedestrians and bicyclists and improving overall community connectivity. Project team members facilitated a mapping activity, breaking participants into smaller groups around large aerial maps and asking them to write down what they wanted to protect, avoid, and create along the South 9th Street Corridor. Each group reported back on their ideas to the rest of the participants.

Facilitators then presented participants with various tools to create an inviting corridor, including examples of various land uses and building types as well as other elements for creating a sense of place within the corridor and improving safety along the corridor. Participants were then asked to identify which areas in the corridor they felt less safe, and what different land use visions they had for the corridor over time. Each group again had the opportunity to report back their ideas to the rest of the participants.

A full summary of comments is available in **Appendix C**.

Walk Audit

The project team hosted a walk audit on March 20, providing community members and interested stakeholders an opportunity to tour a portion of the corridor to discuss and see specific walking and bicycling issues. This provided an opportunity for the group to witness and discuss some of the constraints in the pedestrian and bicycle environments for this portion of the corridor.

Pop-up Event

The team held a Pop-up table at the Salvation Army Day Center on March 20, 2024. This was an opportunity to identify and address concerns for un-housed members of the community who may travel in the Plan Area. Participants wanted to see infrastructure improvements focused on pedestrians and the disabled, beautification and safety improvements, more housing opportunities, support services for the un-housed, and more markets.



What We Heard

Protect

- River Road Market
- Economy and business opportunities for a diversity of businesses
- Vehicle mobility; Two travel lanes on 9th Street
- Logistics industry
- Water quality and habitat on the River
- Air quality

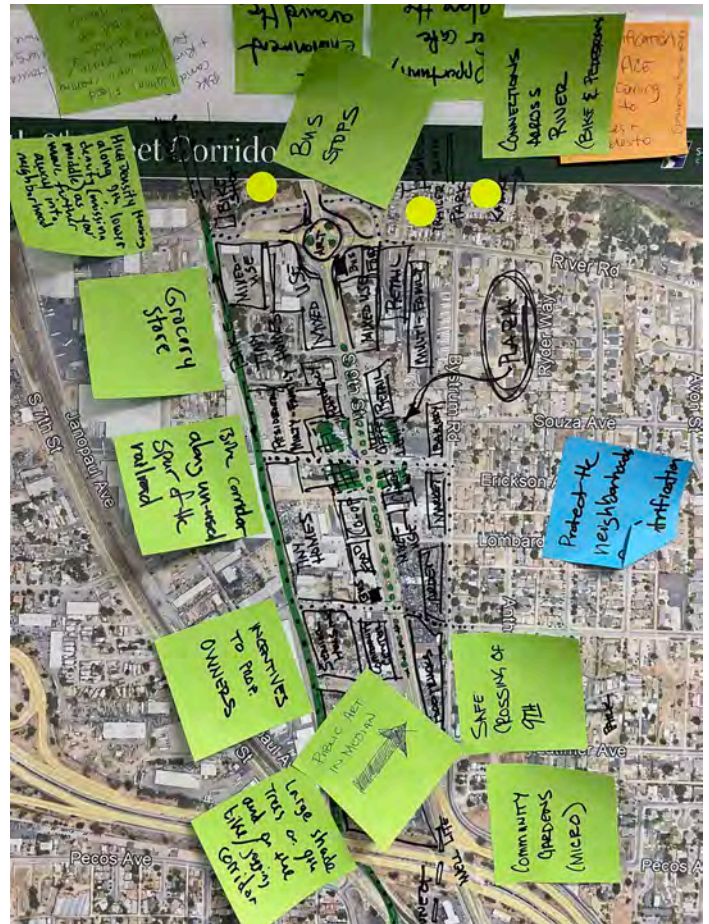
Create

- High-density housing
- Small businesses & grocery stores
- Incentives for property owners for cleanup/ improvements
- Safe crossings on South 9th Street
- Slower speeds
- Safe sidewalks and bike paths
- More bus stops
- Shade trees and landscaping
- Public art
- Traffic signal on Hosmer Avenue
- Infrastructure for semi-trucks
- Lighting: reflectors on the road; street lighting; sidewalk lighting
- Parks

Avoid

- Ugly and dangerous properties
- Heavy semi-trailer traffic on 9th Street, which causes asphalt damage
- Crime
- Illegal dumping
- Narrowing 9th Street

A full summary of comments from all charrette events are available in Appendix D.



Draft Plan Workshop

[Event scheduled for October 16, 2024.]





Chapter 4:

Recommendations

Recommendations

This chapter discusses the recommendations for land use and transportation improvements throughout the South 9th Street Plan Area. The proposed changes to the Plan Area represent a long-term vision of facility improvements and programmatic investment. These recommendations are guided by the Plan's Guiding Principles, simplified below:



Increase and improve connections.



Improve roadway safety for all.



Provide comfortable facilities for walking and biking.



Build green infrastructure.



Improve the appearance of corridor.



Support new land uses.



Increase the sense of personal safety.

Project Development

Along with the guiding principles, projects were identified based on community input, needs identified during evaluation of existing conditions (i.e., collision data, current operations, etc.) projects from previously adopted plans, and feedback from jurisdiction staff.

Proposed Changes in Land Use

Proposed revisions to the existing land uses within the study area as seen in **Figure 12**. These land use recommendations present opportunities to more effectively activate the Plan Area:

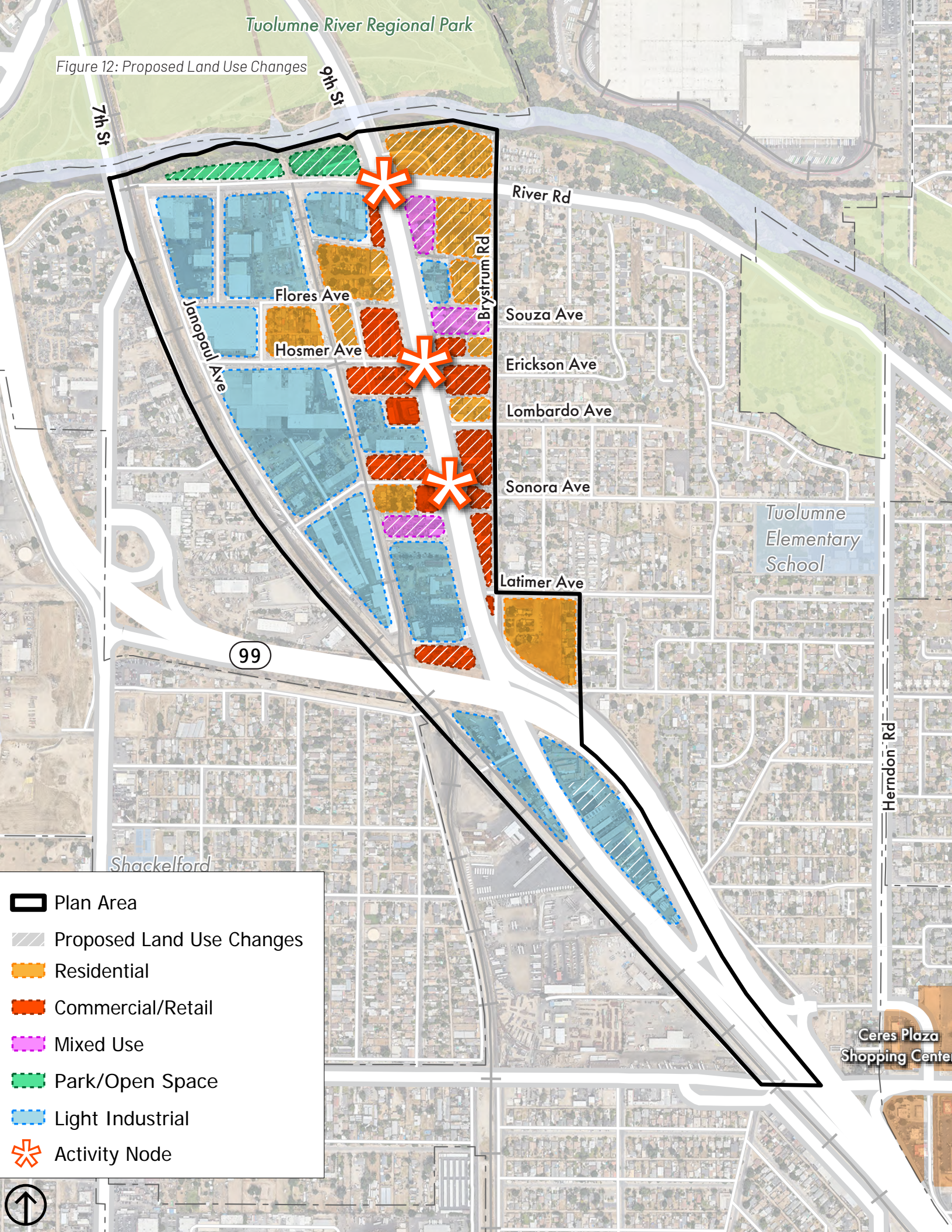
- Transition existing properties along South 9th Street north of SR 99 from industrial to commercial, mixed-use, and residential uses.
- Create new activity nodes with commercial/retail destinations at River Road, Hosmer Avenue, and Sonora Avenue.
- Provide additional housing east of South 9th Street to transition to existing neighborhood east of Bystrum Road.
- Maintain existing residential areas on both sides of South 9th Street.
- Emphasize existing and new light industrial west of South 9th Street and south of SR 99.
- Consider adding a new community park north of River Road.

The following pages depict conceptual retail activity nodes for the River Road, Hosmer Avenue, and Sonora Avenue intersections.

The transportation recommendations in the following section beginning on page 60 are directly linked to the land use changes suggested.

Tuolumne River Regional Park

Figure 12: Proposed Land Use Changes

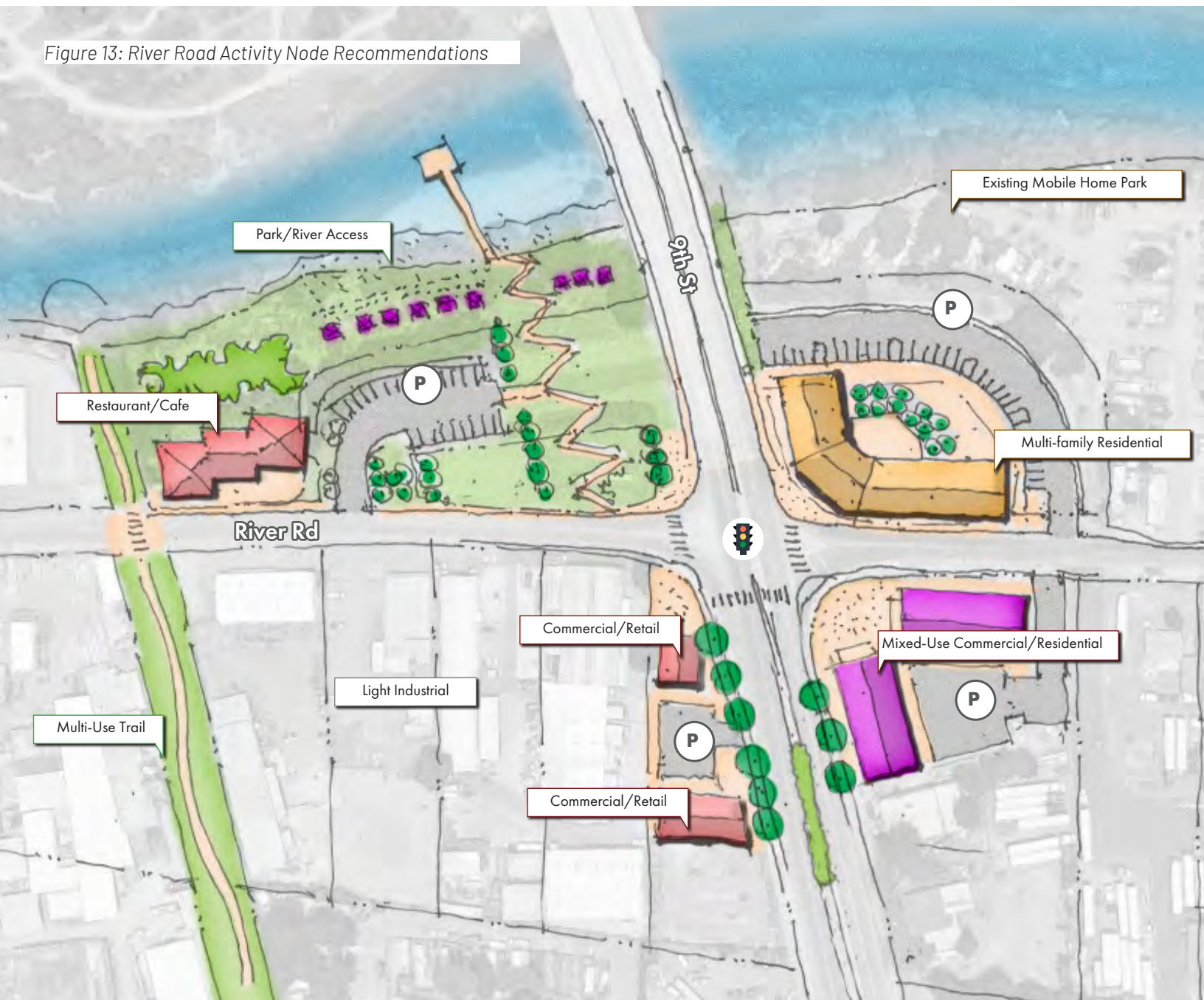


Land Use Recommendations

River Road Activity Node

- A traffic signal at the River Road and South 9th Street intersection would allow the removal of the bridge underpass road. Existing underpass right of way could then be converted into parking.
- North of River Road, a new multi-family development and community park could be constructed with the new activation of the space.
- South of River Road, commercial/retail and mixed use destinations could be considered.
- Potential new multi-use trail using the decommissioned rail spur to connect to the park.

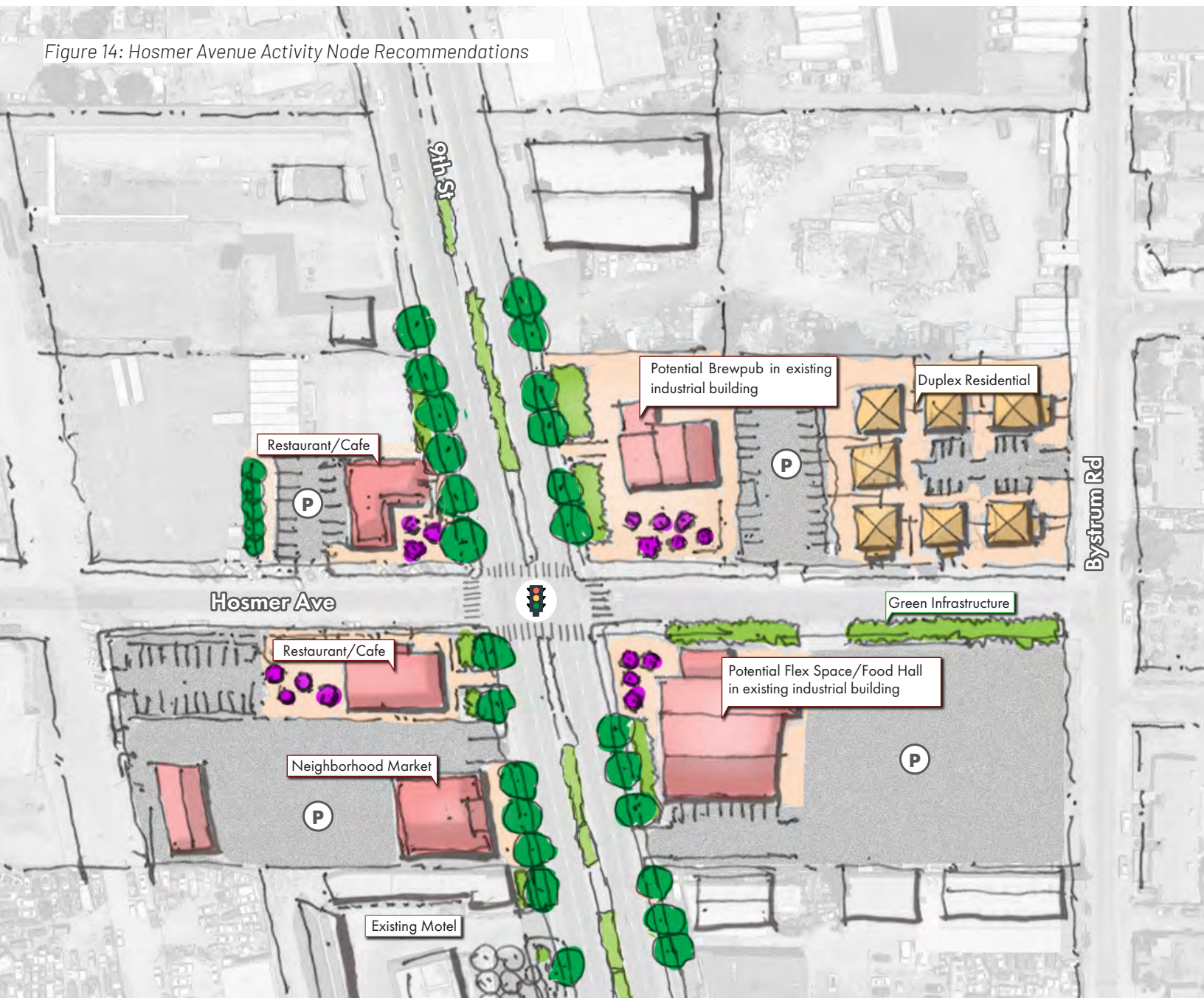
Figure 13: River Road Activity Node Recommendations



Hosmer Avenue Activity Node

- Activate the Hosmer Avenue and South 9th Street intersection with commercial/retail destinations. A new signal and crosswalks would enhance connectivity between these businesses.
- On the east side of South 9th Street, adapt and reuse industrial buildings into commercial uses.
- Along Bystrum Road, construct new medium density residential.
- Integrate green infrastructure on private areas and along public streets.
- Move surface parking lots behind buildings on South 9th Street.

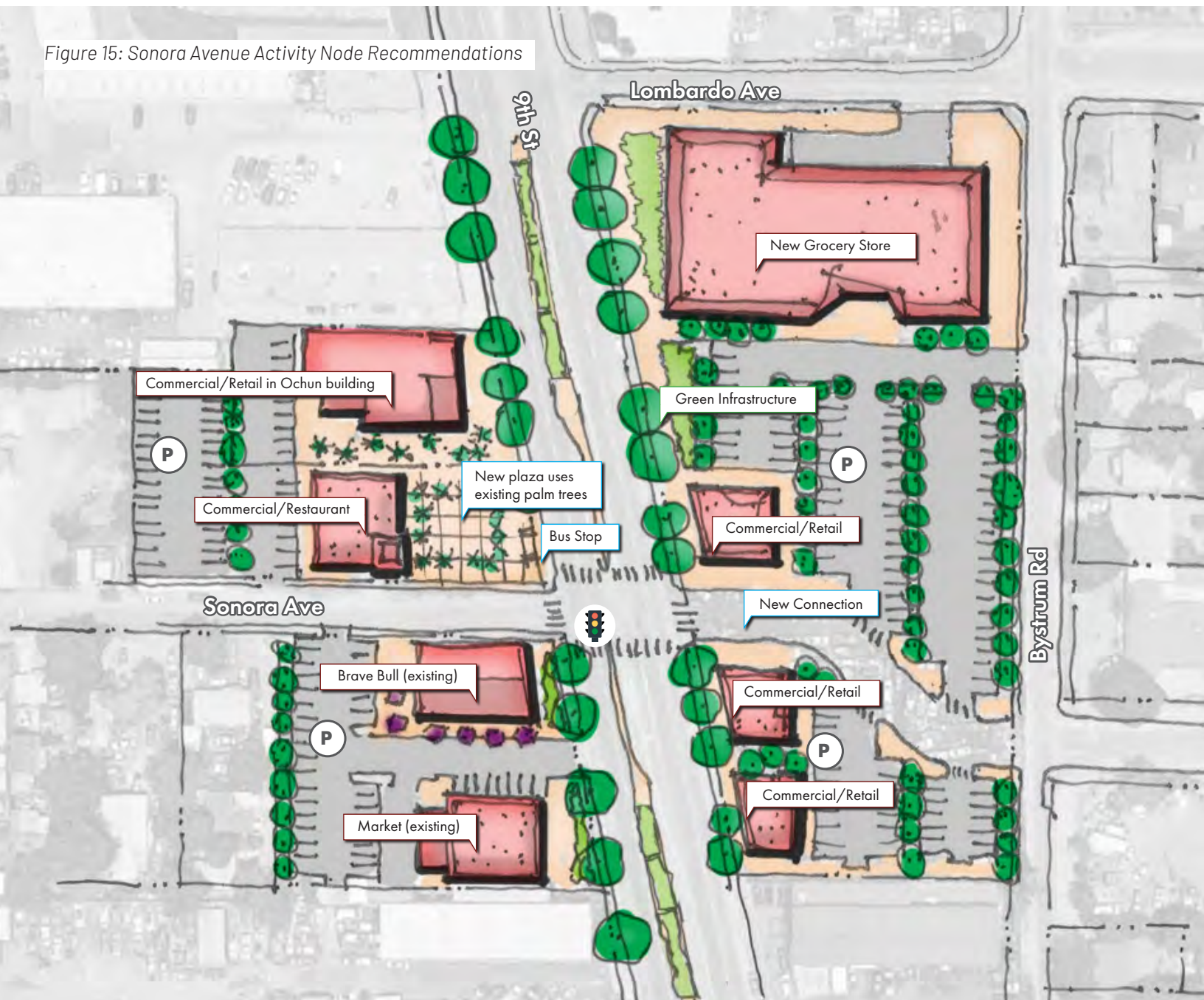
Figure 14: Hosmer Avenue Activity Node Recommendations



Sonora Avenue Activity Node

- A new east-west roadway connection at Sonora Avenue between South 9th Street and Bystrum Road would encourage further connections between commercial areas of South 9th Street and the residential areas east of Bystrum Road. Consider a signal at this intersection.
- Activate intersection with grocery store and neighborhood commercial/retail uses on existing industrial property on east side of South 9th Street. Add retail in existing landmark Ochun building.
- Provide public plazas and outdoor dining. Integrate bus stop with shelter with plaza.

Figure 15: Sonora Avenue Activity Node Recommendations



Land Use Actions

Based on these long-term proposed land use changes and activity node concepts, the following actions are suggested:

- LU-1** Increase code compliance.
- LU-2** Adopt Design Standards for new development, change of use, and major renovations.
- LU-3** Provide incentives for rehabilitation.
- LU-4** Codify land use and zoning recommendations. Allow Multi-family, Mixed Use, and Live-work uses. Zone out Industrial at the northern end of the Plan Area.
- LU-5** Develop opportunities for placemaking, such as a Mural Program.
- LU-6** Inform and support developers from the light industrial commercial sector to encourage and spur investment.
- LU-7** Incentivize combining multiple parcels of land to attract quality development.

Design Guidelines

Purpose and Goals

The South 9th Street Corridor Plan includes goals to make the corridor safer and more inviting. Therefore, the County proposes the following standards that will guide new private development, including additions to existing buildings, will help community revitalization efforts in the form of physical and aesthetic improvements, to support the goals for the Corridor. The intention is that these standards will be used to guide amendments to the Stanislaus County Zoning Ordinance.

Thresholds for Additions

New building projects and major additions for properties along South 9th Street in the plan area should be consistent with the standards in this document. Major additions are defined as follows:

- Additions that total 15% or more of the value of the existing building
- Additions that add 25% or more of square footage to the existing footprint

Standards will be reviewed and approved at staff level.



Building entry facing street with display windows.

Site Design

Building Orientation to Street

Intent: The primary elevation of buildings is the main face of the building, has the most attention to architectural detail, and typically includes the primary building entry. The primary elevation of buildings should be oriented to face South 9th Street, so that they frame the pedestrian environment. On corner lots, the primary elevation should face 9th Street, the secondary frontage should be on the side street.

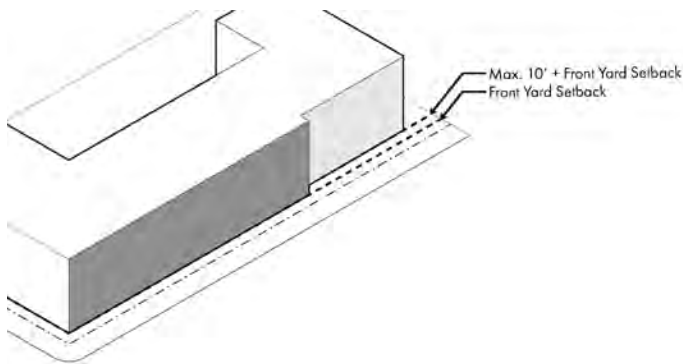
- The main office and visitor entrance should be oriented towards South 9th Street.
- Exception: The main office and visitor entrance may be located on a side of the building if there is a direct pedestrian connection from South 9th Street leading to an entry patio or courtyard area visible from the street.
- A minimum of 50 percent of linear store frontage should be used for the display windows. Windows should be of clear glass. Limited areas of less than 20% of total glass area may include decorative glass and/or glass blocks.
- Auto repair uses and wrecked vehicles should not be visible from the public street. These uses must be screened with fences or landscaping.

Setbacks

Intent: A setback is the distance a building's edge is from the public right-of-way, usually the back edge of the sidewalk. Setbacks determine the relationship between buildings and the public right-of-way and have a significant effect on the pedestrian experience. The most vibrant parts of South 9th Street are where storefronts are close to the sidewalk, providing interest to pedestrians passing by.

Setbacks Along South 9th Street

- Buildings should be located at or within 10 feet of the South 9th Street setback line. Buildings may be set back from South 9th Street property line only as a means to create public spaces such as plazas or entries, display areas, provide a wider sidewalk, or access parking facilities behind the building.



Building frontage setback.

- Allowable uses in the setback area include:
 - Public spaces such as plazas or entries
 - Outdoor dining
 - Extension of public sidewalk space
 - No more than 50% of the front setback should be used as commercial display area, such as for vehicles or other products.
 - Vehicles should not encroach into the public right-of-way, including sidewalks, or landscaped areas, or interfere with vision clearance if located on a corner lot.
- Uses not allowed in the setback space include:
 - Auto repair.

Side and Rear Setbacks

- Where a property is immediately adjacent to an existing single family residential building, any part of the new building within 20 feet of the property line should be setback a minimum of 10 feet above the second floor, so the massing of buildings respects the scale of the surrounding neighborhoods.

Design Standards

Site Access, Parking and Service

Intent: Minimize the impact of driveways and utility services to create a more welcoming and walkable street environment.

Pedestrian and Bicycle Access

- All buildings should be connected to the public sidewalk by a clearly delineated path or walkway.
- Primary routes for pedestrian circulation should provide universal access for people of all ages, size, ability, or disability wherever possible by minimizing the number of steps and elevation changes and providing ADA access to building entries.
- Design cues should be provided along pedestrian connections to help demarcate the transition between public and private spaces. Where there is a pedestrian connection between public and private space, at least one of the following design cues should be used:
 - Change in paving color,
 - Change in paving materials,
 - Landscaping on one or both sides of a walkway.



Bicycle parking, landscape and seating in front setback.

- Buildings should provide secure bicycle parking. Secure bicycle parking should be convenient from the street and should meet one of the following:
 - Covered, lockable enclosures with permanently anchored racks for bicycles;
 - Lockable bicycle rooms with permanently anchored racks; or
 - Lockable, permanently anchored bicycle lockers.

Vehicle Access

- Buildings and parking should be sited to maximize opportunities for shared parking, shared access entries, shared driveways, and to minimize the number of curb cuts on sidewalks along South 9th Street.
- Access points should be limited to the minimum number that is necessary to serve the property. Wherever possible, access driveways should connect to side streets rather than South 9th Street.
- Multiple-lot developments should provide vehicular access to individual lots from an internal street system, rather than creating additional driveways along public street frontages.



Multiple lot development with single vehicle access.

Parking Area Design

- Where parking lots are adjacent to the street or adjacent residential uses, vertical elements, such as low walls, fence, trellises, and landscaping should be incorporated into parking lot edges to make lots attractive and provide screening. However, screening should not obstruct views from a zone 3' to 7' above the ground, to ensure pedestrians can see into the lot for natural surveillance which will deter crime.
- Parking lots should be located behind or next to buildings, not between the building and South 9th Street.
- Parking lots should incorporate trees at intervals to reduce the heat island effect.



Building with parking at side, with parking lot screened by landscaping.

Service and Delivery Areas

- The impact of service, delivery, and storage areas should be mitigated by locating these areas on the sides or backs of buildings, away from public streets and pedestrian circulation.
- On-site queuing space should be provided for vehicles waiting to be unloaded.
- Refuse areas should be screened from public view with appropriate enclosures that incorporate design elements that match proposed buildings and sufficient drainage.
- Refuse areas should be designed to accommodate truck access.
- All loading and pickup should be accommodated fully on site.
- Where property is facing residential across Bystrum Road, loading and refuse should be accessed in the following priority order:
 - Side Street;
 - South 9th Street;
 - Bystrum Road.



Refuse area with screening from public view.

Design Standards

Fences and Walls

Intent: Provide fences and walls that are durable and appealing design components rather than monolithic barriers.

Facing South 9th Street

- To the extent possible, fencing should not be utilized. Where incorporated, there is a preference for see-through fencing that promotes visibility.
 - Solid fences and walls along South 9th Street should not exceed three feet in height within the front setback area along South 9th Street.
 - To ensure visibility to and from the streets, solid fences and walls at or behind the setback line along South 9th Street may be up to seven feet tall. Fences within the required setback should maintain 50% visibility to and from the street at any point over 3 feet in height.
- Chain-link fencing along South 9th Street is discouraged. Where used, chain link fencing should include vinyl slats or similar decorative inserts, or be landscaped to screen the fencing. If landscaping is used, evergreen drought-tolerant upright or columnar shrub species with a maximum height of 8', or flowering vines, should be used.
 - Fencing along South 9th Street should use similar materials, heights, and construction techniques throughout the frontage. These design elements should reflect the material, colors, and design details of the building or buildings on the site.
 - Barbed wire, razor wire, or electric fencing should not be utilized along South 9th Street.



Fencing with over 50% transparency.

Side and Rear Yard Fences

- Fences, walls, or screening may be up to eight feet tall at the side or rear yard, if the fences or walls use similar materials, heights, and construction techniques throughout a development. These design elements should reflect the material, colors, and design details of the building or buildings on the site. On corner properties, no obstruction to view in excess of three feet in height should be placed, built, parked or allowed to grow on any corner lot within a triangular area formed by the street, property lines and a line connecting them at points twenty-five feet from the intersection of the street lines, except that street trees are permitted which are pruned at least eight feet above the established grade of the curb so as not to obstruct clear view by motor vehicle drivers.
- Chain-link fencing visible from public streets is discouraged. Where used, it should include vinyl slats or similar decorative inserts, or be landscaped to screen the fencing.
- Fences or walls that are over 20 feet in length and visible from a public right-of-way should incorporate changes in material, texture, or wall plane every 60 feet maximum.
- Coiled barbed wire, razor wire, coiled barbed wire, or similar type of features are not allowed. Straight barbed wire, while discouraged, is allowed on side and rear yards.
- Electric Fencing is not allowed.

Stormwater Management

Intent: The South 9th Corridor has seen incidents of flooding. Containing stormwater on site or nearby is one means to help alleviate stormwater flow. The following standards are meant to complement and not supersede any existing stormwater management regulations.

- Stormwater detention features should be used to minimize runoff into streets and parking lots. Stormwater detention features include drainage swales and detention basins.
- Stormwater runoff from roofs should be diverted to vegetated swales or detention areas rather than storm drains.



Stormwater bioretention area in parking area.

Design Standards

Building Design

Massing

Intent: New buildings and major additions should face public streets with well composed, welcoming building frontages, or facades, that add to the attractiveness of the area.

Buildings of two stories or more should have a clearly defined base and roof edge so that the façade has a distinct base, middle, and top. Elements to articulate a building's façade should include:

- The top of the building should have one or more of the following:
 - a cornice line with minimum 6-inch overhang;
 - a parapet with minimum 6-inch cap;
 - eaves with brackets or other detailing;
 - upper floor setbacks;
 - and/or sloped roof forms.



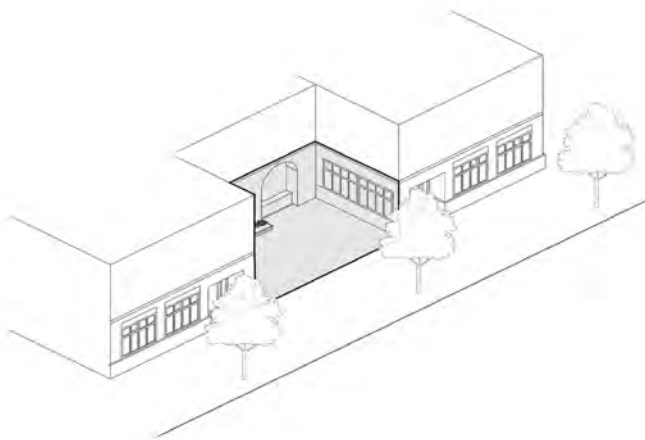
Mixed-use building showing a clearly defined base, middle, and top.

- The middle or body of the building should have a façade made up of regular components including one or more of the following:
 - consistent window pattern;
 - regularly spaced pilasters;
 - recesses;
 - or other vertical elements.
- The base of the building should have one or more of the following:
 - recessed ground floor;
 - a continuous horizontal element at the top of the ground floor;
 - and enhanced window or entry elements such as awnings or canopies. Where pedestrians have access to the base of the building, high quality, durable, and easy to clean materials and finishes should be used, such as stone, brick, cementitious board, glass, metal panels, and troweled plaster finishes.
- Building façades shall establish a human-scaled rhythm with individual building bay widths of 20 to 50 feet. Horizontal or vertical wall articulation shall be expressed through the use of at least one of the following:
 - wall offsets,
 - recessed entries,
 - bay windows,
 - projecting wing walls,
 - roof overhangs,
 - second floor setbacks,
 - canopies,
 - porches,
 - other structural projections.
- Commercial and retail buildings should have architecturally articulated storefronts. Window treatments, awnings and public entries should be designed to promote active use of ground floor businesses.

Entries

Intent: Entries should be visible from the street and welcoming to visitors and passersby.

- Main building entrances should be oriented toward the sidewalk on South 9th Street and include architectural features that give them prominence by using one or more of the following:
 - Building entries should be recessed into entry bays, to create transitional spaces between the street and buildings.
 - Entries covered with projecting roofs, canopies or trellises.
 - Architectural detailing and materials.
- Architectural detailing and materials should be used to distinguish between entries to visitor and employee/service entries.



A recessed entry bay providing a transitional space between the building and street.

Corner Buildings

Intent: Building corners provide additional opportunities for architectural distinction within a streetscape, and treating corners differently than mid-block building forms helps to create landmarks and provide identity for corridors like South 9th Street.

- Where corner properties exist on 9th Street, at least one of the following special elements shall be integrated at the corner to activate and energize the intersection:
 - Special architectural and design features facing the corner, such as taller building elements or prominent architectural detail.
 - A small setback to provide a public plaza with direct access to the building.
 - Special or unique landscape treatment.
- Where feasible, the main entrance of a corner building shall be located at the corner.



Corner building with prominent architectural details and openings facing the street.

Design Standards

Riverfront Development

Intent: The riverfront area of South 9th Street has an opportunity to become an asset if properly developed.

- Properties developed along the river should have a public component to them, opening the river to public access available to residents and visitors to the neighborhood.
- Properties developed along the river should accommodate anticipated flood levels to ensure public safety.

Roof Design

Intent: Provide roof designs that contribute to visual interest along South 9th Street while allowing for durability and economy.

- Flat roofs should include a cornice or parapet detailing. Sloped roofs should include detailing around eaves.
- Roof-mounted mechanical, electrical, and external communication equipment, such as satellite dishes, HVAC equipment and microwave towers, should be screened from public view from South 9th Street.
- For new townhouse and similar dwelling group developments, no more than four side-by-side units may be covered by one unarticulated roof. Variation may be accomplished by changing the direction of slope, and by including elements such as dormers.

Windows

Intent: Windows should prioritize visibility into and from buildings along the street, adding to the safety and security of the area.

- For commercial and retail buildings, on façades that face a public street, windows that provide views into active interiors should be used, and long stretches of blank walls should be avoided wherever possible.
- For residential buildings, windows should have an architectural distinction that reflects the residential character of the building. Residential building walls along South 9th Street should have windows at all floors above ground level.
- For mixed-use buildings, windows patterns should architecturally distinguish a building's first floor retail character, with a higher percentage of windows than on upper floors.



Commercial building articulated by canopies over windows.

Building Details and Materials

Intent: Promote materials, finishes and colors that are substantial, long lasting and increase the visual quality of individual businesses and public frontages.

- Where there are two or more buildings on a site, building materials shall be selected to complement the site and adjacent development. The use of similar materials on buildings provides a strong link that unifies varying architectural features of multiple buildings.
- In commercial and mixed-use buildings, selective use of awnings and canopies is encouraged as an integral part of the building design to provide shelter for pedestrians, an element of scale and visual interest, and solar shading for windows and storefronts. Signage on awnings shall be limited to the awning flap or valance.

Lighting

Intent: To ensure that the lighting design of new buildings and major additions provide enough lighting to ensure public safety while not intruding onto neighboring properties or into the sky with glare.

- Lighting shall be designed to minimize off-site glare or spill onto neighboring streets or properties.
- Parking lots shall be designed with a greater number of shorter, low wattage, tightly spaced fixtures rather than a lesser number of taller, higher-wattage fixtures.
- Building-mounted lights should be limited to those needed for safe and secure building access. External building light fixtures should be fully shielded and directed downward. High intensity building mounted security lights without a shield and not directed downward are strongly discouraged.

Design Standards

Landscaping

Intent: Landscaping provides opportunities to enhance both the public right-of-way and private property, and contribute to environmental quality by reducing stormwater runoff, casting shade, improving air quality, and providing visual relief. As South 9th Street is within the City of Ceres sphere of influence, Ceres Water Efficient Landscape Guidelines should be adhered to in addition to the guidelines below.

- Provide a variety of landscaped spaces that accommodate different activities and needs, including buffering from sidewalks, at the perimeter of parking areas, and seating and dining areas,
- Incorporate landscaped corner plazas, courtyards and other street-level open spaces to identify and establish special locations in the area.
- Plant materials should be native California or non-invasive, drought tolerant species adaptable to the Central Valley climate, with exceptions for higher water using plants used for limited focal point landscaping, such as at entry points or gateways. .
- Trees should be planted in and around parking lots to provide shade and visual relief.
- All plants located adjacent to buildings, sidewalks, pathways, curbs, roads or other obstructions should be installed to accommodate their minimum spread at maturity.
- The palette of plant materials in landscape buffers should create an aesthetically-pleasing space through a mix of colors, heights and types of plants.
- Landscaped areas shall be regularly maintained to keep them aesthetically pleasing, and to remove dead and dying plants.

Signage

Intent: To ensure that signage along South 9th Street is attractive and communicates well without being overbearing. All signage shall adhere to Chapter 26 - Signing Standards in the Ceres Zoning Code.

- Signage will mostly consist of addresses and names of businesses. Signs advertising products or businesses not at the subject address are discouraged.
- Signage should be integrated into the architecture of the building or landscape fronting the street.
- Monument signs larger than 32 square feet in area and six feet in height, or pole mounted signs are discouraged.
- Internally illuminated (cabinet) lighting is strongly discouraged. All lighting should be externally illuminated or backlit individual letters.
- Signs should not obscure architectural elements on buildings such as windows, rooflines or vertical elements such as columns.

CPTED Standards

Intent: Crime Prevention through Environmental Design (CPTED) is a crime prevention philosophy based on the theory that the proper design and effective use of the built environment can lead to a reduction in the fear and incidence of crime, as well as an improvement in the quality of life.

CPTED is effective because of the concept of “defensible space.” This concept suggests that space in the human environment is defensible, and if it is or appears to be guarded, it is less likely that a crime or unsafe event would happen there. There are four strategies to help with this below, with associated design guidelines for each:



Development with window and door openings facing the street.

Natural Surveillance (Eyes On The Street)

Natural surveillance is the design of an area that places physical features, activities, and people in locations that maximize the ability to see what is occurring in a given space. An example of natural surveillance is an open area visible from a sidewalk or street. This visibility allows pedestrians and motorists passing by to see into the area and detect activity there. New development should follow these guidelines:

- Adjacent to public streets and open spaces, design landscaping to provide visibility and properly trim and maintain ground landscaping to a maximum height of 36 inches and trees to a minimum height of 7 feet which allows clear visibility to view the area.
- Use a greater number of appropriately scaled lighting fixtures to illuminate the pedestrian environment, rather than a few taller fixtures. Pedestrian-scaled lighting along walkways should be no higher than 20 feet and spaced no farther than 30 feet’.
- New development shall orient a minimum of one window or door opening onto each side of the development that borders a public street or open space.

Design Standards

Territorial Reinforcement

Territorial reinforcement is the design of an area that clearly defines its boundaries and ownership. All space can be defined as public, private, or semi-public/semi-private. The underlying principle of territorial reinforcement is that the transition between spaces should be clearly identifiable for both the user and others in the area. Territorial reinforcement allows legitimate users to develop a sense of ownership over a space and act as guardians.

New development should provide the following:

- Decorative fencing or landscaping should be placed around the semi-private outdoor patio of a business or residence adjacent to a public street or open space. The fencing or landscaping should be no greater than 36 inches.
- Proper signage that communicates the ownership of a space and the rules of its use should be provided where businesses use outdoor space.



Territorial reinforcement in the form of fencing placed around the semi-private outdoor patio of a business.

Access Control

Access control is the physical guidance of movement to and from a space by the placement of entrances, exits, fencing, landscaping, locks, and other barriers. This CPTED strategy works because it not only limits and guides movement, but it also causes improper access to be noticed more readily. The following should be provided in new development:

- Provide separated and well-marked pedestrian pathways through parking lots and private open spaces, which give direction to the users and create a safer path of travel by alerting observers to the concentrated presence of pedestrians.

- Place bollards across pedestrian entrances to private or public open spaces to prevent vehicle entry but allow pedestrian entry. If needed, bollards may be made removable for emergency vehicle access.
- Limit the number of pedestrian entries into a private or public open area to no more than two along each street frontage, to control access and provide better observation of who is entering and leaving a space.



Access control in the form of bollards placed across pedestrian entrances to private or public open spaces.

Design Standards

Maintenance

Maintenance is the upkeep of an area or building. It demonstrates that someone cares about a space, is watching, and will defend the property against crime. Routine maintenance or clean-up can have a great deal of impact in making an area unattractive to illegitimate activities. To create a perception of responsibility and caring in neighborhoods, property owners, tenants, and residents should follow the following guidelines:

- New development should provide signage indicating who is responsible for the physical maintenance of a development or business.
- Broken windows, and debris and litter should be immediately addressed.
- Broken down equipment, inoperable vehicles, and outdoor material storage should be screened from view of the public.
- Landscaping should be maintained.



An example of a well-maintained property.

COMPLETE
AUTO
REPAIR

BRAKE & LIGHT
TEST for DMV
SMOG CHECK
HABLAMOS ESPAÑOL

SMOG
STATION



Best Buy Auto Glass
209-521-2002

Transportation Toolkit

This Plan recommends a variety of transportation infrastructure improvements. Additional active transportation infrastructure types are below. Walking facilities include sidewalks, crosswalks, and shared-use paths. Bicycling facilities, in order from lowest stress and most comfortable to highest stress and least comfortable, include shared-use paths, separated bikeways, bike lanes, and bike routes.

Pedestrian Facilities

Sidewalks

Sidewalks are paved areas immediately adjacent to the vehicular right-of-way for the use of pedestrians. Sidewalks may be used by people riding bicycles unless prohibited.

Crosswalks

Crosswalks are street crossings for pedestrians and may be marked or unmarked. Marked crosswalks feature striping and other enhancements. These features may be used to raise awareness of the crossing and to delineate the best place to cross. There are two types of marked crosswalks:

- **Controlled crosswalks** are located with stop signs or traffic signals.
- **Uncontrolled crosswalks** are located without stop signs or traffic signals. Under California law, drivers are legally required to yield to pedestrians at uncontrolled crosswalks.

Additional features can be added to crosswalks to increase visibility on busy streets:

- **High-visibility crosswalk** markings add additional striping to the pavement.
- **Warning signage** improves visibility of crosswalks and increases the likelihood that a driver will yield to or stop for pedestrians.
- **Curb extensions** decrease the pedestrian crossing distance at intersections and improve the visibility of pedestrians waiting to cross the street.
- **Median refuge islands** allow pedestrians to cross one direction of traffic, then wait in the center of the street to cross the other direction of traffic.
- **Rectangular rapid flashing beacons (RRFBs)** allow the pedestrian to activate a flashing light when crossing.
- **Pedestrian hybrid beacons (PHBs)** require traffic to stop for pedestrians when activated, but allow vehicles to proceed with caution after the pedestrian crossing has been completed.



Transportation Toolkit

Bicycle Facilities

Shared-Use Paths

Shared-use paths (Class I), often referred to as bike paths, are off-street facilities that provide exclusive use for non-motorized travel by bicyclists and pedestrians.

Separated Bikeways

Separated bikeways (Class IV), also known as cycle tracks, are physically separated bicycle facilities that are distinct from the sidewalk and designed for exclusive use by bicyclists.

Bike Lanes

Bike lanes (Class II) are on-street facilities that use striping, stencils, and signage to denote preferential or exclusive use by bicyclists.

Buffered bike lanes have an additional painted section of pavement that provides space between bike and motor vehicle travel lanes.

Bike Routes

Bike routes (Class III) are streets with signs and optional pavement markings where bicyclists travel on the shoulder or share a lane with motor vehicles.

Bike Parking

Bicycle parking encourages ridership by supporting the final stage of a bicycle trip. At locations with high ridership, both short-term and long-term parking should be accommodated.

Short-term bicycle parking, such as bike racks, is intended for visitors, allowing patrons to park their bike for short periods. Bike parking should be located in well-lit areas and near building entrances to discourage theft. Long-term bicycle parking is intended for employees and residents to protect bicycles for extended periods. Long-term facilities are more secure, including bike lockers, bike cages, and bike rooms.



Traffic Calming

Traffic calming can include a wide range of design treatments capable of reducing vehicle speeds and thus improving the safety and comfort of the transportation network for all users. Traffic calming devices may employ vertical deflection, such as speed bumps, or horizontal deflection such as curb extensions.

Wider roads are associated with greater crash rates and higher impact speeds. Narrowing roadways often leads to decreased vehicle speeds and improves safety. Restriping narrower travel lanes for vehicle traffic can reduce motor vehicle speed. Narrow lanes can make room in the roadway right-of-way for painted medians, center turn lanes, bicycle lanes, or parking.

Wayfinding

Wayfinding signage can be used to direct users to key destinations and connecting facilities. Wayfinding signs can also encourage pedestrians and bicyclists to visit local businesses. These signs provide the most value at intersections of key bicycling and walking routes. Chapter 9B of the California MUTCD provides guidance on sign design and installation. These standard signs may be augmented by signs depicting distances in miles to encourage walking and bicycling. Class III bike routes designations and signage can also be used to assist with wayfinding on roadways without other marked bike facilities.



Transportation Toolkit

Green Infrastructure

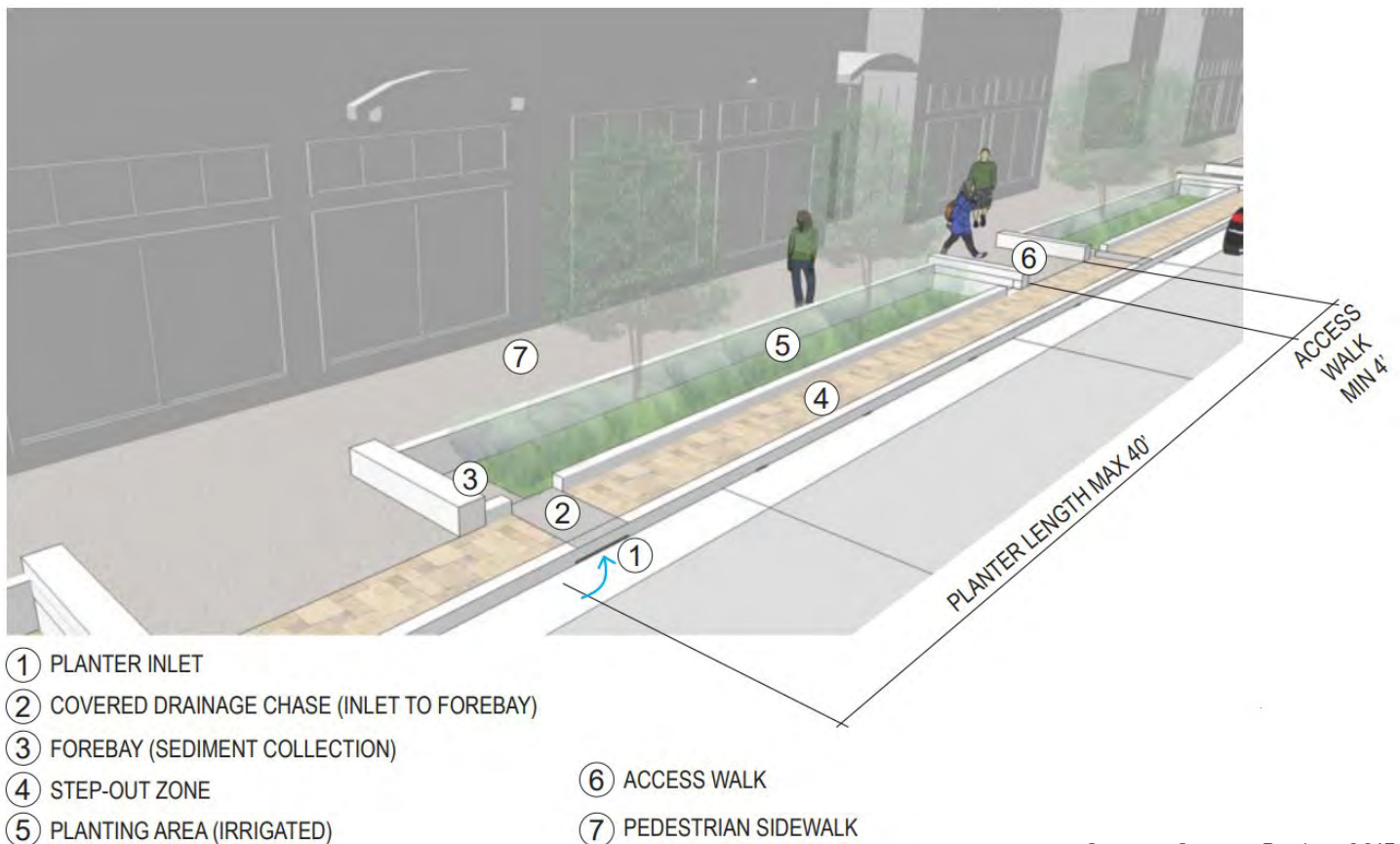
Green infrastructure plays a crucial role in urban environments by managing stormwater, enhancing infrastructure resilience, and improving aesthetics. These systems utilize natural processes to capture, filter, and slowly release rainwater, reducing the risk of flooding and alleviating pressure on conventional drainage systems. By integrating vegetation and soil, they also help mitigate urban heat islands and provide essential habitats for local wildlife.

Effective stormwater management ensures that nearby land and roads are less susceptible to damage from runoff and flooding, thereby reducing maintenance costs and disruptions. Bioswales and stormwater planters can be strategically placed along roadways, offering dual benefits of environmental enhancement and infrastructure resilience.

Additionally, green infrastructure contributes to pedestrian and cyclist comfort by creating more aesthetically pleasing and safer travel environments.

Thus, green infrastructure not only supports effective stormwater management but also enhances the functionality and livability of transportation systems.

For more information on Green Infrastructure, as well as example guidelines, see the City of Denver Public Works' Ultra-Urban Green Infrastructure document.



Source: Stream Design, 2015.

Lighting

Sufficient lighting can improve safety for all road users. Well-lit streets reduce the risk of collisions due to decreased visibility. Lighting can also reduce the fear of crime. This increase in perceived security can encourage pedestrian activity at night, specifically at and near transit stops.

Thoughtfully designed lighting can beautify urban landscapes and create a welcoming atmosphere. Pedestrian walkways should have lighting that allows people to identify faces from about 30 feet away.

For more information on making lighting decisions that benefit pedestrian safety, see FHWA's [Pedestrian Lighting Primer](#) (2022).



Transportation Recommendations

The following transportation recommendations present opportunities to enhance the transportation network through roadway improvements and new facilities for pedestrians and bicyclists. Phasing is introduced in the following figures to guide the order in which infrastructure projects are completed.

Intersections

Due to the high traffic volumes and long distances between existing crossings along South 9th Street, the corridor would see improved circulation from the addition of more traffic signals to provide more control and crossing opportunities.

The following intersections of South 9th Street could benefit from new traffic signals to improve traffic operations and safety:

- River Road
- Hosmer Avenue
- Sonora Avenue (if east leg connected)
- SR 99 westbound off-ramp

Traffic signals could be coordinated to optimize vehicle throughput while maintaining lower speeds along the corridor. Installing signals at these intersections, shown in **Figure 16**, come with the co-benefit of providing crossing opportunities for pedestrians and bicyclists, as discussed in the following section.

The existing signalized intersections at Latimer Avenue and Pecos Avenue could also be upgraded with Leading Pedestrian Intervals (LPIs) to give pedestrians additional time to cross.

To address mobility goals for the corridor, additional intersection reconstruction would be needed at the SR 99 westbound off-ramp. To slow northbound speeds exiting SR 99 onto South 9th Street, the northbound slip lane of the SR 99 westbound off-ramp should be closed and replaced with a ramp approach closer to a perpendicular angle. This modification would decrease the turning radius of vehicles and slow drivers, as shown below.

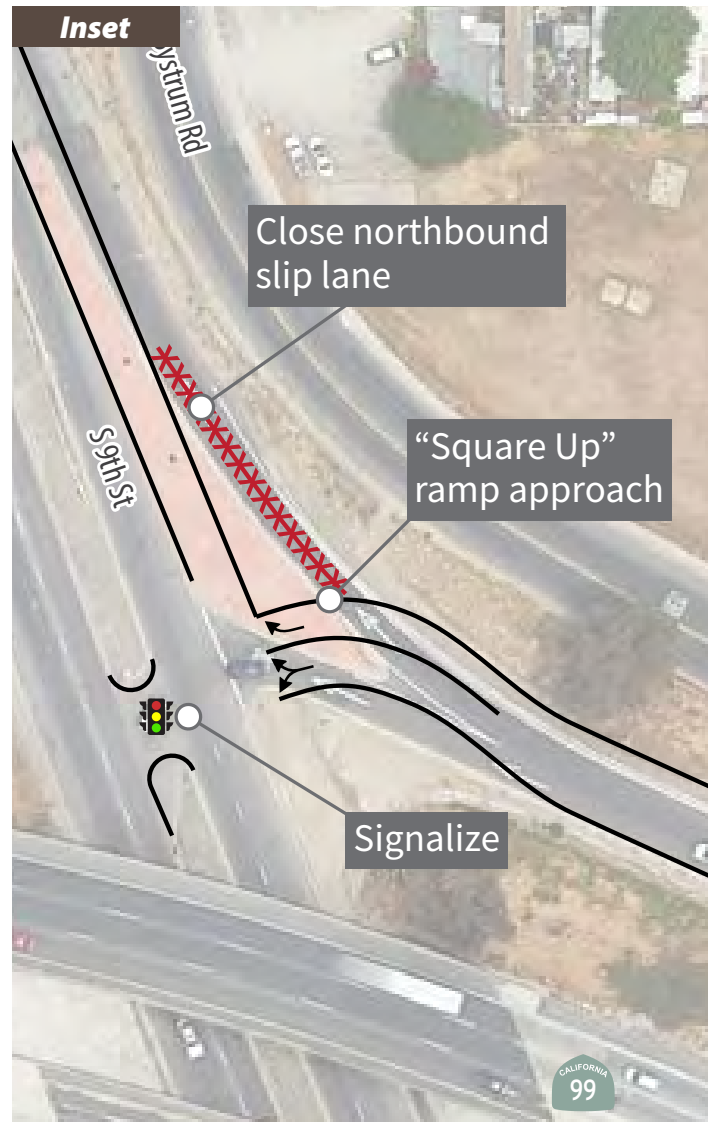
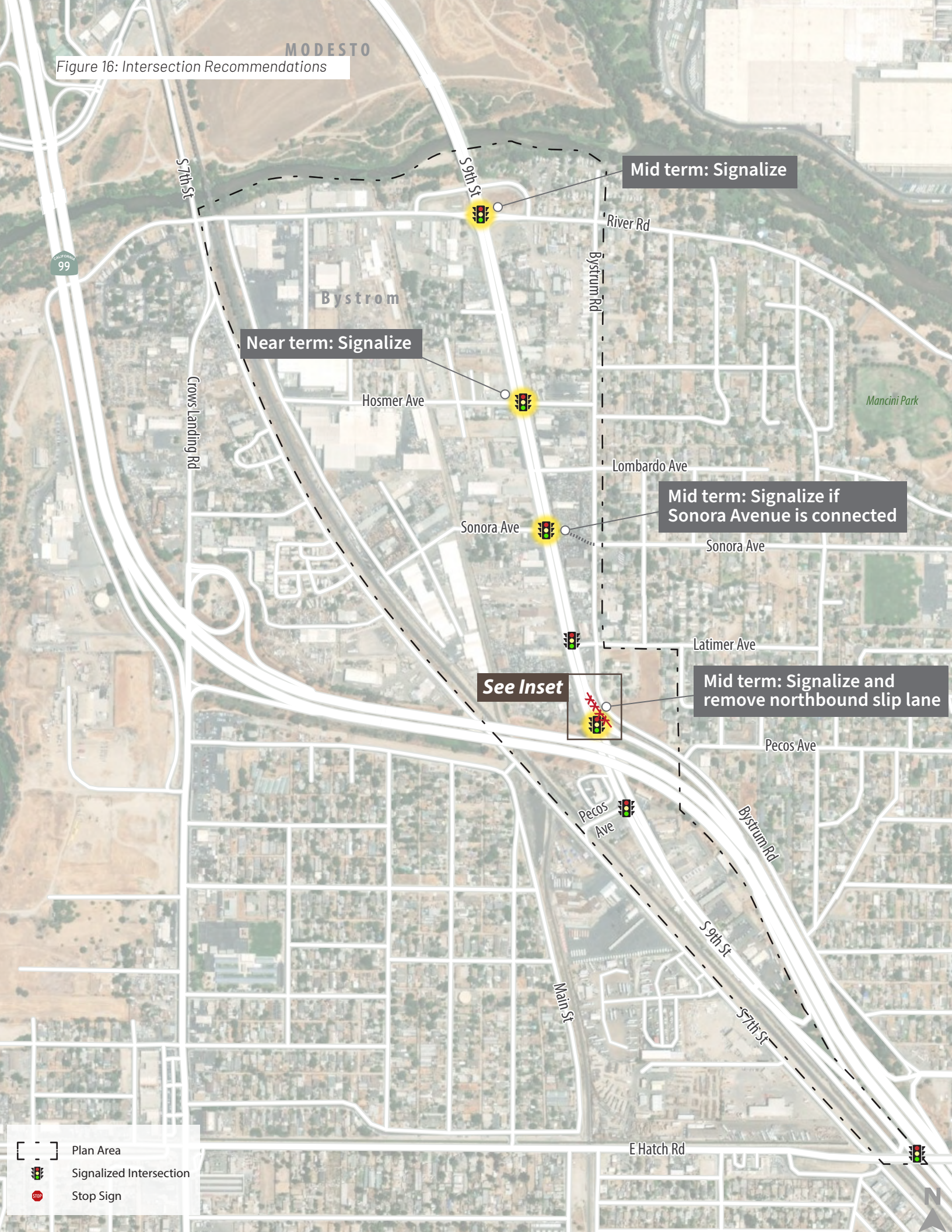


Figure 16: Intersection Recommendations



Mid term: Signalize

Near term: Signalize

Mid term: Signalize if Sonora Avenue is connected

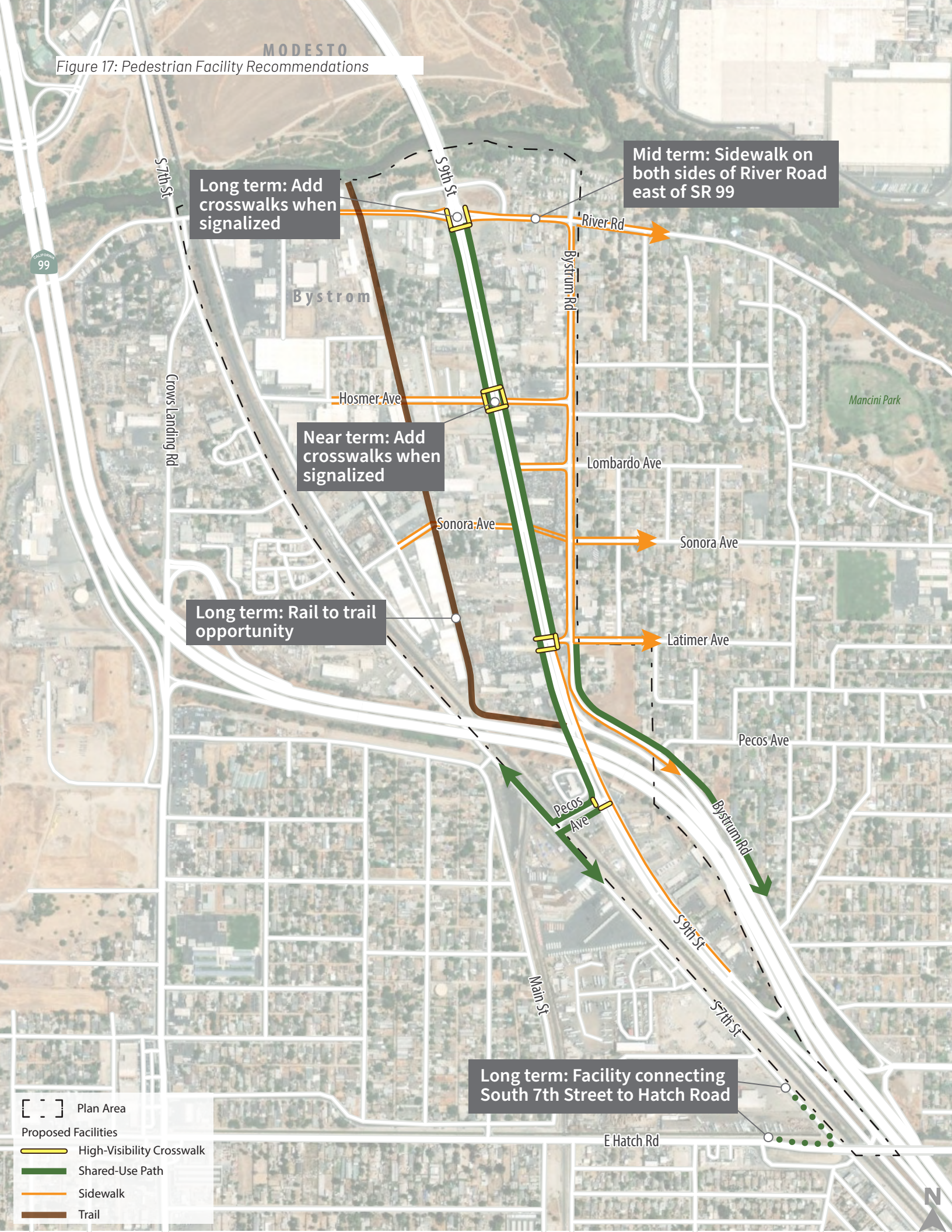
See Inset

Mid term: Signalize and remove northbound slip lane

- Plan Area
- Signalized Intersection
- Stop Sign



MODESTO
Figure 17: Pedestrian Facility Recommendations



Long term: Add crosswalks when signalized

Mid term: Sidewalk on both sides of River Road east of SR 99

Near term: Add crosswalks when signalized

Long term: Rail to trail opportunity

Long term: Facility connecting South 7th Street to Hatch Road

- Plan Area
- Proposed Facilities**
- High-Visibility Crosswalk
- Shared-Use Path
- Sidewalk
- Trail



Walking Facilities

There are a substantial amount of pedestrians that frequent the corridor. In order to further improve the pedestrian experience, high-visibility crosswalks, sidewalks, and shared-use paths are recommended throughout the corridor as shown in **Figure 17**.

Sidewalks are recommended on both sides of the street on River Road, Hosmer Avenue, Lombardo Avenue, Sonora Avenue, and Latimer Avenue to fill existing gaps in the pedestrian network.

A wide pedestrian walking area/shared-use path along both sides of South 9th Street would provide an inviting space for residents, employees, and visitors alike. This shared-use area should connect to the existing sidewalk on the South 9th Street Bridge on the north side and to Pecos Avenue on the south side.

Pecos Avenue should be developed and maintained as a safe and convenient multimodal connection between South 9th Street and South 7th Street across the Union Pacific (and soon to be ACE) rail tracks.

With signalization, the intersections of South 9th Street at River Road, Hosmer Avenue, and Sonora Avenue will also benefit from the addition of new marked crosswalks. Sidewalks on the corridor's side streets would increase the number of east-west connections.

Where signals are not viable, such as at the intersection of River Road & Bystrum Road, rectangular rapid flashing beacons (RRFB) and high-visibility crosswalks can be installed. *FHWA's Guide for Improving Pedestrian Safety at Uncontrolled Crossing Locations* provides information on how to select the most appropriate treatment for a specific location.

Figure 18: Bicycle Facility Recommendations



Long term: Rail to trail opportunity



Mid term: Add wayfinding signage to direct bicyclists away from South 9th Street south of Latimer Avenue



Mid term: Use wayfinding to direct cyclists to use Pecos Ave and 7th Street

- [- - -] Plan Area
- Proposed Facilities
- Multi-Use Path (Class I)
- Bike Lane (Class II)
- Buffered Bike Lane (Class IIB)
- Bike Route (Class III)



Biking Facilities

The South 9th Street corridor is often utilized by bicyclists to travel between downtown Modesto and Ceres as well as into residential neighborhoods.

Bicycle facility recommendations are shown in **Figure 18**. To create a better bicycle connection to downtown Modesto and the Tuolumne River bike trail, buffered bike lanes are recommended on the South 9th Street Bridge, starting north of River Road.

In the primary segment of South 9th Street from River Road to Latimer Avenue, shared-use paths are recommended on both sides of the corridor, as discussed in the previous section and shown in **Figure 17**. South of Latimer Avenue, shared-use paths are also recommended on the west side of South 9th Street until Pecos Avenue, where bicyclists should be routed to South 7th Street to avoid the SR 99 on- and off-ramps closer to Hatch Road. Wayfinding signage should be utilized to assist bicyclists navigating off of the corridor.

Extending off of Latimer Avenue to the south, the east side of Bystrum Road could also be used to connect to shopping and other amenities on Hatch Road.

East of South 9th Street, residential-serving bike routes along River Road, Bystrum Road, Hosmer Avenue, and Sonora Avenue, as well as bike lanes along Latimer Avenue would further enhance the biking experience and facilitate greater interactions between residents and the corridor.

Lastly, the existing rail spur between Janopaul Avenue and South 9th Street could be leveraged as a “rail to trail” opportunity, potentially even connecting to the Tuolumne River.

Recommended Cross-sections

Following the evaluation of existing conditions and hearing from the public and the Advisory Group about their priorities, three alternative cross-sections were developed for the South 9th Street corridor. These corridor alternatives are shown in Appendix E. Due to the varied width of right-of way in the Plan Area, the corridor was split into two areas on consideration: (A) River Road to Pecos Avenue, and (B) Pecos Avenue to SR 99 On-ramp.

The project team asked the Advisory Group to provide feedback on which alternatives for both segments were preferred in consideration of their vision of the corridor. The cross-section on the following page was nearly unanimously selected as the preferred alternative.

Several cross-sections for Hosmer Avenue and Bystrum Avenue were also discussed.



A

Figure 19: Recommended South 9th Street Cross-section (River Road to Pecos Avenue)



Features of the preferred alternative for this segment include:

- Wide curb-height shared-use area for bikes and pedestrians (with the option for a bike travel area delineated with paint, color, or varied texture pavement)
- Street trees on either side of the road
- Median street lighting and landscaping
- Curb side street furniture and stormwater retention
- No street parking

The second cross-section represents South 9th Street from Pecos Avenue to the SR 99 On-ramp. Features of the preferred alternative for this segment include:

- Wide raised shared-use area for bikes and pedestrians
- Median street trees
- Side street lighting and wayfinding elements
- Street parking on the eastern side of the road

B

Figure 20: Recommended South 9th Street Cross-section (Pecos Avenue to SR 99 On-Ramp)

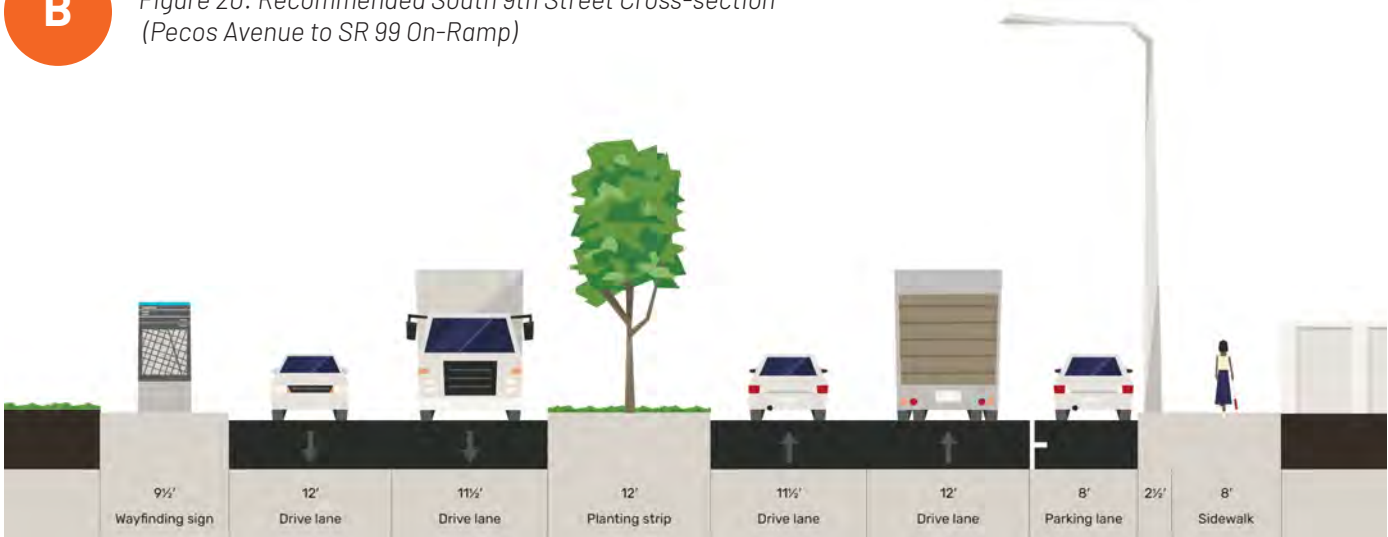


Figure 21: South 9th Street Render (facing northbound)

This rendering shows the potential build out of South 9th Street



Street trees and stormwater retention

Raised center medians with street lights and landscaping



Wide shared-use areas

Bus turn-ins

Quick-build & Interim Opportunities

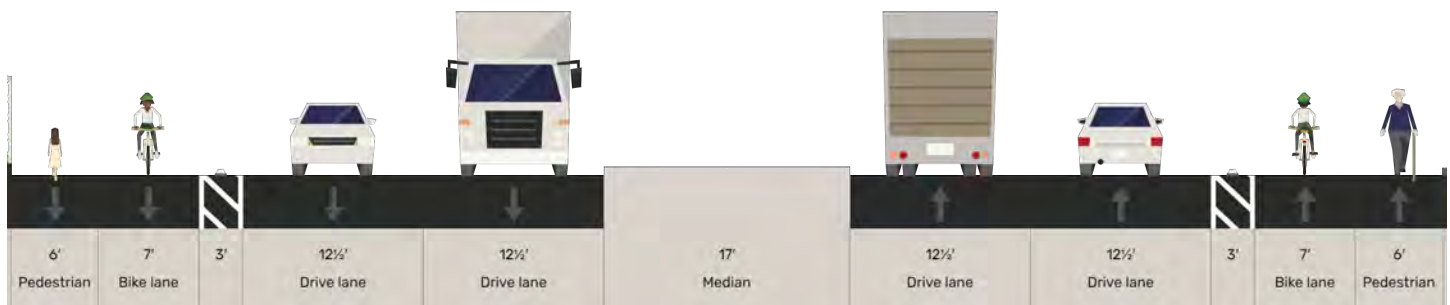
Stanislaus County Public Works currently has funds to perform re-striping along South 9th Street, but not the full build-out of the corridor.

In consideration of this near-term opportunity, the following cross-section is suggested:

- Reduction in lane widths: Reduce existing lane widths to be 11 - 13 ft wide to reduce excessive travel space but still accommodate large vehicles.
- Buffered bike lanes: Consider adding a 2 - 4 foot buffer and 6-8 foot bike lanes to replace the underutilized parking lane.

Keep in mind that the quick-build treatment shown in the cross-section below is intended to accommodate with the existing funding realities, and is not intended to be the final outcome of the corridor.

Figure 22: South 9th Street Interim Cross-section



At intersections, there are also opportunities to use low-cost materials such as paint and bollards to reduce vehicle turning radii and shorten pedestrian crossing distances.



Source: NYC DOT, 2022.

Non-Infrastructure Programs

Local programs can supplement the recommended improvements to the built environment. These efforts can be led by local non-profits, community organizations, Stanislaus County, or one of the many other partners of this Corridor Plan. Many of these programs have proven effective in other Central Valley communities. Other effective countermeasures, particularly focused on roadway safety, can be found in *Countermeasures that Work*, 11th Edition (National Highway Transportation Safety Administration, 2023): <https://www.nhtsa.gov/book/countermeasures/countermeasures-that-work>

Education & Encouragement

A variety of school-centered active transportation programs can increase safety and convenience for students and parents. An example are walking and biking school buses where kids and families walk or bike to school in groups. Establishing such a program when more children live in the Plan Area in close proximity to Tuolumne Elementary School would encourage walking.

For adults, as more commercial/retail businesses arise on the corridor, local events such as ‘shop small Saturdays’ or small temporary markets could encourage walking in a safe and casual environment.

Enforcement

The Stanislaus County Sheriff’s Department regularly monitors activity in the South 9th Street area. Community members have repeatedly expressed concerns with crime, including assault, theft, and alcohol- and drug-related crimes. Apprehension towards personal safety in the Plan Area is prevalent.

Continuing enforcement efforts periodically, and expanding to places frequented by pedestrians, has been shown to be an effective tool in creating safer streets. Efforts of the Sheriff’s Department could be made more effective by implementing principles from CPTED, previously discussed in the Recommendations chapter, to provide more passive safety benefits.



Walking School Bus in Washington D.C.
(Source: <https://ggwash.org/view/90958/how-and-why-to-start-a-walking-school-bus>)

Maintenance

The County does not have formal policies for maintaining bicycle and pedestrian facilities or landscaping. However, street sweeping is done regularly. Restriping occurs when markings become worn or during concurrent pavement treatment or repairs on Stanislaus County streets.

Establishment of a Maintenance Assessment District (also called Lighting and Landscape Maintenance Districts or Community Benefit Districts) would help assure that regular maintenance is used to keep facilities accessible, safe, and attractive. This District could create a formal maintenance policy that addresses both incidental and periodic maintenance of green infrastructure, vegetation, and the transportation network. To establish a Maintenance Assessment District, property owners must vote to assess themselves to pay and receive County services beyond what the County normally provides. The Stanislaus County Public Works Engineering Division provides for the establishment and maintenance management of various service districts.

Mural Program

Mural Programs typically pay local artists to paint large paintings on empty walls or large neglected spaces. Murals can foster civic pride and identity, as well as boost tourism and local economies by attracting visitors and supporting local artists. Murals can celebrate cultural heritage, promoting community engagement and dialogue. Additionally, murals can contribute to crime reduction by transforming neglected areas into visually appealing, well-maintained spaces.

For an example of a mural program, see Sacramento's Wide Open Walls: <https://www.wideopenwalls.com/>





Source: Wide Open Walls Sacramento





Chapter 5:
Implementation

Implementation

A list of recommended infrastructure improvements are summarized in Table 1 on page 84 and mirror the recommendations in Figures 12 - 20. These walking, biking, and driving improvements represent the long-term vision of this corridor and Plan Area.

Land Use

Land Use implementation requires investment by property owners and business owners who see the benefit in creating improvements along South 9th Street. Outreach during the Corridor Plan process indicated that there is a strong desire in the community and from stakeholders for new land uses along the corridor, including for retail stores, restaurants, and housing. Chapter 4 includes several concepts for properties along South 9th Street that show how new development opportunities and new land uses could improve the corridor. Design Standards that will help guide these new buildings or major additions to ensure corridor improvement are also included in Chapter 4.

As new development occurs over time, it will accommodate and potentially help fund street and infrastructure improvements. These street and infrastructure improvements will in turn catalyze and encourage more new development as property owners and businesses see the increasing value of locating on South 9th Street.

The primary means of funding new development and associated corridor improvements will be private financing, but there are programs to assist with funding. Housing is a statewide need and there is funding available for housing development that includes affordable housing, including Low-income Tax Credits. Infrastructure Infill grants are also available through the State.

In order to capture some of the private investment from new development to improve infrastructure and streetscape there are several mechanisms that could be explored. Developers could be required to improve the streetscape bordering their property. Development impact fees could be collected to help fund future improvements. An Enhanced Infrastructure Financing District could divert certain property taxes towards area improvements, or an Assessment District or Landscaping and Lighting Maintenance District could finance improvements and continued maintenance. Any district financing programs would need to be approved by a majority of property owners.



Transportation

Infrastructure improvements include sidewalks, crosswalks, shared-use paths, separated bikeways, bike lanes, and bike routes. The proposed networks are designed to connect neighborhoods to key destinations and to serve as recreational assets.

Implementation of planned facility improvements is anticipated to occur:

- Through grant funding pursued to implement this plan;
- In conjunction with adjacent land development projects; and
- In conjunction with maintenance and capacity enhancement projects, such as slurry seals, pavement reconstruction, and striping projects.

Implementation of each project is dependent upon availability and acquisition of funding; projects requiring land acquisition or utility relocation require extra time to implement. Improvements associated with work on adjacent roadways or development of adjacent land uses provides opportunities for implementation relatively easily or at lower cost than if implemented separately. In these cases, lower priority improvements may be implemented before higher-priority improvements, depending on the location of these land development and roadway projects. Implementation of each project is also dependent on detailed feasibility and design studies. Stanislaus County should periodically update this plan to reflect evolving needs and progress toward completion.

Costs

High-level cost estimates of the infrastructure projects are provided in **Appendix F**. Costs may be higher depending on the extent to which utilities need to be relocated or land acquired to implement facilities. However, some facilities may be implemented during development of adjacent land uses or in conjunction with other projects. Therefore, some of these costs will not be directly borne by the County.

Project cost estimates are based on unit cost estimates. These estimates were developed based on relevant project experience in the area. Note that these cost estimates are high-level, therefore more detailed study and design of individual project will be required to refine them.



Table 1. Recommended Infrastructure Projects

Location	Description
Roadway Improvements	
South 9th St Plan Area	Pavement Rehabilitation, re-striping, & lighting
South 9th St from River Road to Pecos Avenue	Re-construction to narrow raised center median
Intersection Improvements	
South 9th St & River Road	Re-design and signalize Intersection, marked crosswalks, curb ramps
South 9th St & Hosmer Ave	Signalize, marked crosswalks and curb ramps
South 9th St & Sonora Ave	Signalize, add marked crosswalks and curb ramps
South 9th St & Latimer Ave	Modify signal to include Leading Pedestrian Interval, modify curb ramps
South 9th St & SR-99 WB Off-Ramp	Square up ramp approach, Signalize, marked crosswalks and curb ramps
Bicycle & Pedestrian Facility Improvements	
South 9th St from River Rd to Pecos Ave; Pecos Ave from South 9th St to South 7th St	Class I Shared/Multi Use Path
South 9th St Bridge	Class IIB Buffered Bike Lane
Latimer Ave from South 9th St to Avon St	Class II Bike Lane
Bystrum Rd from River Rd to Latimer Ave; River Rd from South 9th St to Herndon Ave; Hosmer Ave from South 9th St to Bystrum Rd; Sonora Ave from Bystrum Rd to Herndon Ave	Class III Bike Route
River Road from rail spur to Avon St; Hosmer Ave from Flores Ave to Bystrum Rd; Lombardo Ave from South 9th St to Bystrum Rd; Sonora Ave from Janopaul Ln to South 9th St; Latimer Ave from South 9th St to Avon St	Sidewalk
South 9th St	Wayfinding Signage
Green Infrastructure Improvements	
South 9th St; Hosmer Ave	Streetside stormwater planters
South 9th St	Street trees and tree wells

Proposed Schedule

Realizing the vision of the South 9th Street Corridor will require years to complete. Below represents a list of recommended key actions for Stanislaus County to undertake in the next twenty years and beyond.



Near-term

Land Use:

- Regularly engage in code enforcement activities.
- Identify and adopt priority Design Standards for new development, changes of use, and major renovations.
- Inform and support developers from the light industrial commercial sector to encourage and spur investment.

Transportation:

- Complete the re-paving and striping project with existing funding, and implement other recommendations identified in this plan where possible.
- Pursue funding for quick-build roadway treatments.
- Begin engineering design of the South 9th Street Corridor and apply for funding for the construction of the corridor.
- Work with Caltrans to advance re-design of the South 9th Street & Northbound SR 99 Off-ramp intersection.

Mid-term

Land Use:

- Codify land use and zoning recommendations from this Plan.
- Provide incentives to local businesses for rehabilitation.
- Develop opportunities for placemaking, such as a Mural Program.

Transportation:

- Advance engineering designs for South 9th Street and begin construction.
- Establish a maintenance district in the South 9th Street Plan Area.

Long-term

Land Use:

- Continue near-term and mid-term activities to build upon the commercial character of the corridor and residential area in the eastern portion of the Plan Area.

Transportation:

- Complete construction of South 9th Street infrastructure improvements.
- Maintain roadway treatments and green infrastructure.

Funding Sources

Multiple federal, state, regional, and local funding sources are available for bicycle and pedestrian projects and programs that the County can apply for. A full resource table is provided on the next page. Some of the funding sources most relevant to this plan include the following:

The **Active Transportation Program (ATP)** consolidates diverse transportation initiatives into a single program with an annual budget of around \$120 million from state and federal sources. ATP aims to increase walking and biking trips, enhance safety for non-motorized users, support regional greenhouse gas reduction efforts, promote public health, and provide a range of projects benefiting various user groups, including disadvantaged communities.

Frequency Biennial

The federal **Congestion Mitigation and Air Quality (CMAQ) Improvement Program** allocates funds to states for transportation projects aimed at alleviating traffic congestion and enhancing air quality, especially in regions of the country struggling to meet national air quality standards.

Frequency Annual

Highway Safety Improvement Program (HSIP) is a Federal-aid initiative designed to achieve a significant reduction in traffic fatalities and serious injuries across all public roads, including non-State-owned roads and tribal land. California's Local HSIP focuses on infrastructure projects with recognized crash reduction benefits. Funding can be used for preliminary engineering, right of way, and construction.

Frequency Annual

Sustainable Transportation Planning Grants are offered by Caltrans to encourage local and regional planning goals that support the implementation of RTP/SCS projects. These funds can be used for a variety of focused community planning projects, including those that support rural active transportation, temporary demonstration projects, and community needs assessments.

Frequency Annual

The **Safe Streets and Roads for All (SS4A)** grant program has a budget of \$5 billion in appropriated funds spanning from 2022 to 2026. The SS4A program supports regional and local endeavors through grants to prevent roadway fatalities and severe injuries. Stanislaus County was recently awarded an SS4A planning grant to develop a Safety Action Plan and engage in several supplemental planning efforts. Recommendations made in the upcoming Safety Action Plan should be consistent with the recommendations made in the Plan to the extent feasible in order to be eligible for SS4A implementation funding.

Frequency Annual

The **Surface Transportation Block Grant Program (STBG)** provides flexible funding for a variety of transportation projects. The federal government provides funding through the STBG Program to each of the states for road, transit, active transportation, and other transportation needs. In California, these funds are administered by Caltrans. Caltrans distributes a portion of these funds to regional agencies like StanCOG.

Frequency Annual

Local Development Fees collected on land development projects can provide match funding or full implementation of projects where there is a nexus to the project.

Frequency Ongoing

Federal and State Earmarks also present an opportunity to secure funding at both the federal and state level. Earmarks often have short timelines for consideration; proactively creating fact sheets with funding needs and benefits of potential projects can support engagement with Congressional Representatives and State Assembly members and Senators.

Frequency Annual

Other typical local funds in Stanislaus County that could supplement these project funding sources through maintenance and local matches include the Highway Users Tax Account (HUTA), Measure L, and Senate Bill 1 Road Maintenance and Rehabilitation Account (RMRA) funds.

Table 2. Funding Sources

LOCAL FUNDING SOURCES		
Surface Transportation Block Grant Program (STBG)	Infrastructure	https://www.fhwa.dot.gov/specialfunding/stp/
Congestion Mitigation and Air Quality Improvement Program (CMAQ)	Infrastructure	https://arb.ca.gov/resources/documents/congestion-mitigation-and-air-quality-improvement-cmaq-program
STATEWIDE FUNDING SOURCES		
Affordable Housing and Sustainable Communities (AHSC)	Infrastructure	https://sgc.ca.gov/programs/ahsc/
Active Transportation Program (ATP)	Infrastructure & Non-Infrastructure	https://catc.ca.gov/programs/active-transportation-program
Clean California (Clean CA)	Non-Infrastructure	https://cleancalifornia.dot.ca.gov/
Local Highway Safety Improvement Program (HSIP)	Infrastructure	https://dot.ca.gov/programs/local-assistance/fed-and-state-programs/highway-safety-improvement-program
Local Partnership Program (LPP)	Infrastructure	https://catc.ca.gov/programs/sb1/local-partnership-program
Promoting Resilient Operations for Transformative, Efficient, and Cost-Saving Transportation (PROTECT)	Infrastructure	https://dot.ca.gov/programs/local-assistance/fed-and-state-programs/protect
Road Maintenance and Rehabilitation Account (RMRA) & Highway Users Tax Account (HUTA)	Infrastructure	https://sco.ca.gov/aud_road_maintenance_sb1.html
Solutions for Congested Corridors Program (SCCP)	Infrastructure	https://catc.ca.gov/programs/sb1/solutions-for-congested-corridors-program
Sustainable Transportation Planning (STP) Grant	Infrastructure	https://dot.ca.gov/programs/transportation-planning/regional-planning/sustainable-transportation-planning-grants
FEDERAL FUNDING SOURCES		
Rebuilding American Infrastructure with Sustainability and Equity (RAISE)	Infrastructure	https://transportation.gov/RAISEgrants
Strengthening Mobility and Revolutionizing Transportation (SMART)	Infrastructure	https://transportation.gov/grants/SMART
Safe Streets and Roads for All (SS4A)	Infrastructure	https://transportation.gov/grants/SS4A
State Transportation Improvement Program (STIP)	Infrastructure	https://catc.ca.gov/programs/state-transportation-improvement-program